

**TO: HONORABLE CHAIRMAN AND PLANNING COMMISSION**

**FROM: RON WHISENAND, COMMUNITY DEVELOPMENT DIRECTOR**

**SUBJECT: PLANNED DEVELOPMENT 04-024**  
**2805 THEATRE DRIVE, APNS: 009-851-023 AND 009-023-016**  
**(APPLICANT – LEE MONSON AND LEE WEBB)**

**DATE: FEBRUARY 27, 2007**

**Needs:** For the Planning Commission to consider an application for a Planned Development to construct a 120 unit hotel and 20 cottage suites.

- Facts:**
1. The project site includes two properties located at the western edge of the City off of Theatre Drive on the north and south side of Nutwood Circle. The 120 unit hotel is proposed on the south side of Nutwood Circle, and the cottages are proposed across the street to the north. See Attachment 1, Vicinity Map.
  2. The proposed hotel and cottage project is intended to be a high-quality destination resort, featuring attractive architecture and many site amenities.
  3. The properties are currently vacant, and are surrounded by on the west and south with residential development, and commercial uses to the north and east of the cottage site.
  4. The property is zoned Highway Commercial – Planned Development (C2-PD), and is designated as Regional Commercial (RC) in the General Plan. Hotels are a permitted uses in the C2-PD zoning district.
  5. There are several oak trees located on the project site, which are proposed to be preserved. Tree protection measures are incorporated into the project as recommended in the Arborist Report prepared for this project.
  6. The project incorporates more than the required on-site parking spaces for the hotel and appurtenant uses, and the cottages site. The proposed building height for the hotel component of the project ranges from 38 feet for the majority of the building, and up to 48 feet in height for the tower end features. The cottages are only proposed to be one and two stories in height, up to approximately 20 feet. The height limit for the in C2 zone is 50 feet.
  7. The Development Review Committee (DRC) reviewed this project on August 22, 2005, and recommended approval to the Planning Commission. This project has been on hold pending interim improvements constructed at the Highway 46 West and 101 interchange.
  8. An environmental review was conducted for this proposed project, in accordance with the California Environmental Quality Act (CEQA), and no significant environmental impacts were identified that would result from this project with mitigation measures incorporated. Therefore, a draft Mitigated Negative Declaration has been prepared for consideration.

**Analysis**

and

**Conclusions:** The proposed hotel project is divided into two sites: the 120 unit hotel located on the south side of Nutwood Circle; and the 20 unit cottage suites on the north side of the street. The architectural theme, landscaping palate and general quality of site ornamentation is proposed to be consistent between the two sites. The proposed architectural style incorporates an elegant European quality with Mediterranean elements, as represented in the tower design and porte-cochere primary entrance features. The fenestration throughout utilizes multi-pane lighted windows, many with arched details. Blue window awnings are proposed to accent the main hotel window and building facades. The applicant proposes to use ornamental wrought iron and stone details, as well as use of tile and metal roof materials. As previously noted, the main hotel is proposed to be three and four stories in height, but within the permitted building height for the C2 zoning district.

A specific hotel operator has not been identified. It will be important that any future developer or tenant of the project retain the quality of design and materials, and not expect the ability to make significant changes to fit corporate design features. A condition of approval has been added to ensure adequate notice of the City's desire that this become a high quality destination resort.

The site design for both planning areas incorporates a looped driveway system. The parcel in front of the hotel site (which has frontage on Theatre Drive) is mostly vacant with a temporary mobile home sales use on the corner. The hotel project will need to extend the driveway east to Theatre Drive for additional site safety access. The front parcel is planned to be developed in the future as an extension of uses that would be complimentary to the hotel project and designed with a cohesive site plan between the two properties for reciprocal access.

Most on-site drainage is proposed to be retained on the property through use of lagoon system, which is planned to include fountain features and to be an integral part of the site landscaping. A large open grass area is planned between the two hotel wings, and includes a pool, spa and cabana. This area will help buffer the proposed building from the adjacent mobile home park located to the south and west of the site.

A manager's caretaker unit is proposed in the rear area of the hotel site to provide on-site management and maintenance. The parking areas required for the project are proposed around the loop driveway areas adjacent to the southern and western property lines, as well as some spaces in the front area of the site. The parking spaces which comply with the City's parking regulations are proposed to be set back 20 feet from the southern property line, and a few feet along the western boundary. Landscaping is proposed within the parking lot setback areas. A six foot masonry stucco perimeter wall is proposed around the both project sites to provide additional buffering between the hotel and cottages and adjacent land uses.

The existing oak trees are proposed to remain and be protected. The trees are planned to add to the aesthetics of the site by being incorporated into the landscape areas. The project Arborist Report includes tree protection measures which will be included in the Conditions of Approval for this project. The landscape plan proposes a few palm trees as landscape accents. Palm trees do not typically fit in within the "north county" regional landscape palate, as they do not flourish well in our cold winter climate and appear out of place in a landscape dominated with oak trees, sycamores and other native species.

The project would need to tie into the City's water and sewer services through lines in Nutwood Circle to Theatre Drive. City standard frontage improvements will be required along the project frontage on Nutwood Circle.

Per the requirements of the California Environmental Quality Act, an environmental review was conducted for this project. The primary issue of concern identified relates to potential traffic impacts. This project has been held back from moving forward through the planning entitlement process until interim improvements were constructed at the Highway 46 West and 101 interchange. Earlier in the process, the projects traffic impacts were determined to exceed the capacity at the interchange with all of the other development that has been approved in the interchange impact area. Recently, interim capacity increasing and safety enhancing improvements have been completed at the interchange. While these improvements have helped the traffic problems at the interchange, they have not solved all of the congestion and delays issues. The City's General Plan identifies long term improvements needed at the interchange in order to mitigate the traffic problems at that location. The City has been actively pursuing implementing those improvements by the purchase of properties to be used in the long term improvements and initiation of assembling the collection of fees for the Highway 46 West Assessment District that all projects in the impact area have been participating in. These assessments will be applied toward the cost of the long term improvements. In the short run, (until the long term improvements are completed, estimated to be complete in 2009), the proposed project will tip the level of service to an LOS E for evening peak hours. However, as noted, these impacts are currently being mitigated. Therefore, by the time this project is completed and operating (if approved), the LOS at the interchange is projected to be within an acceptable range of LOS C, in compliance with the City's General Plan.

**Reference:** Paso Robles General Plan and EIR, Paso Robles Zoning Ordinance, Downtown Design Guidelines, 2006 Paso Robles Economic Strategy and CEQA.

**Fiscal Impact:** None.

**Options:** After opening the public hearing and taking public testimony, the Planning Commission is requested to take one of the actions listed below:

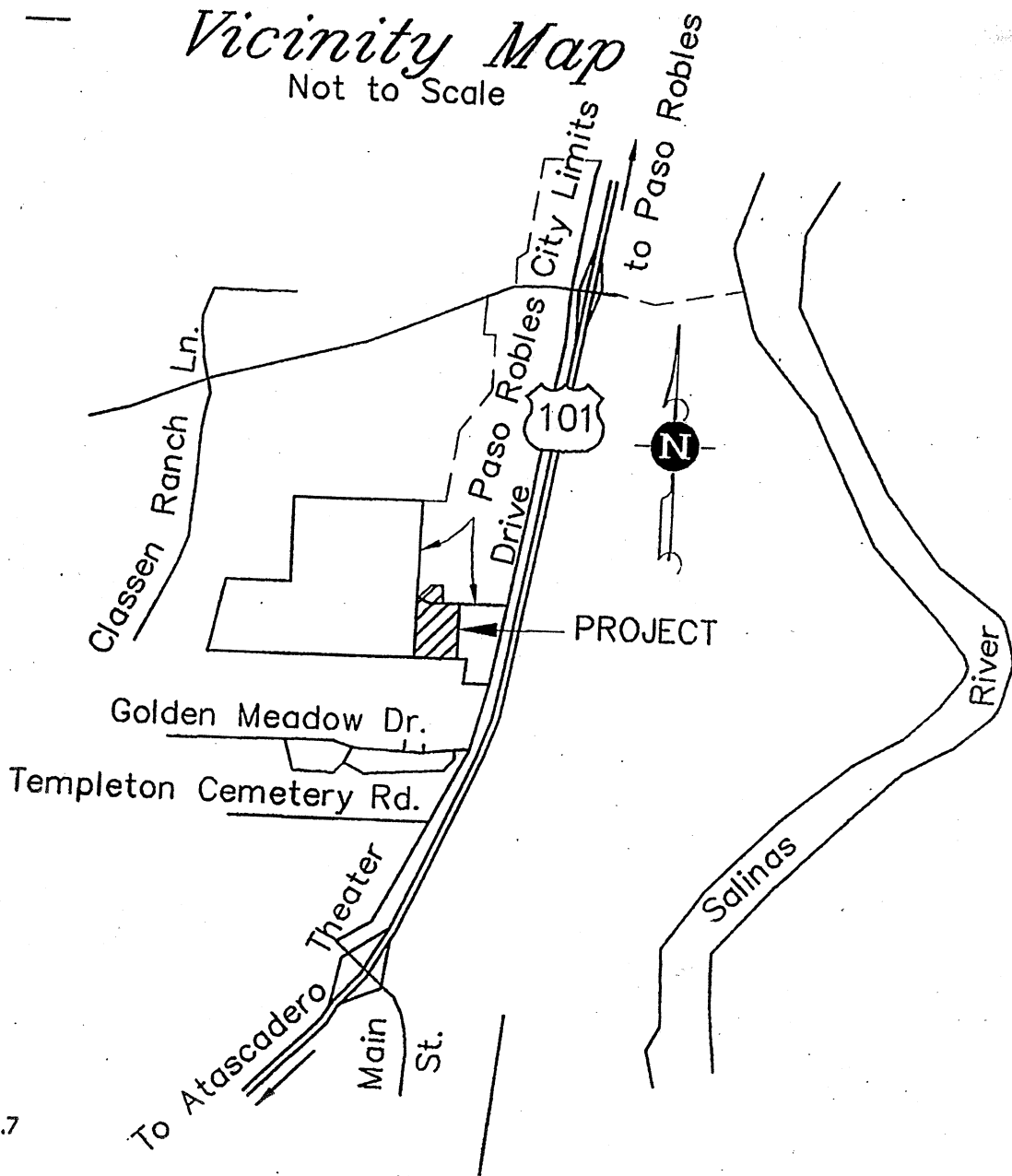
By separate motions:

- a. (1) Adopt the attached Resolution issuing approving the Mitigated Negative Declaration; and (2) approve Planned Development 04-024.
- b. Amend, modify, or reject the above-listed action.
- c. Request additional information and analysis.

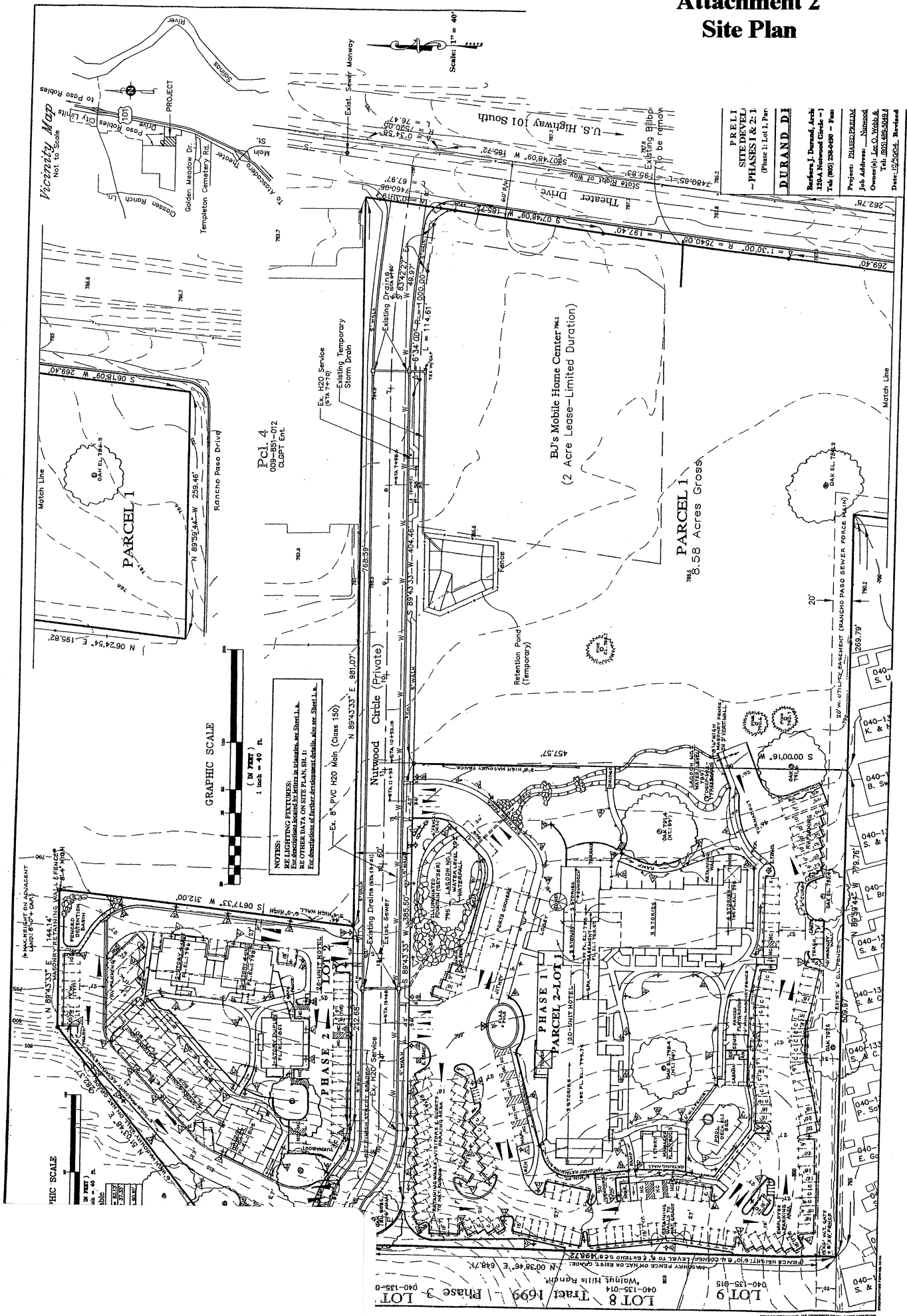
**Staff Report Prepared By:** Susan DeCarli

**Attachments:**

1. Vicinity Map
2. Site Plan
3. Elevations and Landscape Plans
4. Arborist Report
5. Traffic Study
6. Memorandum from City Engineer
7. Resolution to Adopt the Mitigated Negative Declaration
8. Resolution to Approve PD 04-024
9. Newspaper and Mail Notice Affidavits
10. Initial Study
11. Project Description prepared by applicant



# Attachment 2 Site Plan



**Vicinity Map**  
Not to Scale

Parcel 1  
8.58 Acres Gross

Parcel 2-Lot 1  
120-Unit Hotel

Parcel 3-Lot 1  
120-Unit Hotel

Retention Pond (Temporary)

Nutwood Circle (Private)

Rancho Paso Drive

U.S. Highway 101 South

Graphic Scale  
1 inch = 40 ft.

Notes:  
1. ALL DIMENSIONS ARE IN FEET.  
2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.  
3. ALL DIMENSIONS ARE TO CENTERLINE UNLESS OTHERWISE NOTED.  
4. ALL DIMENSIONS ARE TO CENTERLINE UNLESS OTHERWISE NOTED.  
5. ALL DIMENSIONS ARE TO CENTERLINE UNLESS OTHERWISE NOTED.

Scale: 1" = 40'

Parcel 4  
009-851-012  
CLGPT ENT.

Ex. 8" PVC H2O Main (Class 150)

Ex. H2O Service  
Existing Temporary  
Storm Drain

Parcel 1  
8.58 Acres Gross

Parcel 2-Lot 1  
120-Unit Hotel

Parcel 3-Lot 1  
120-Unit Hotel

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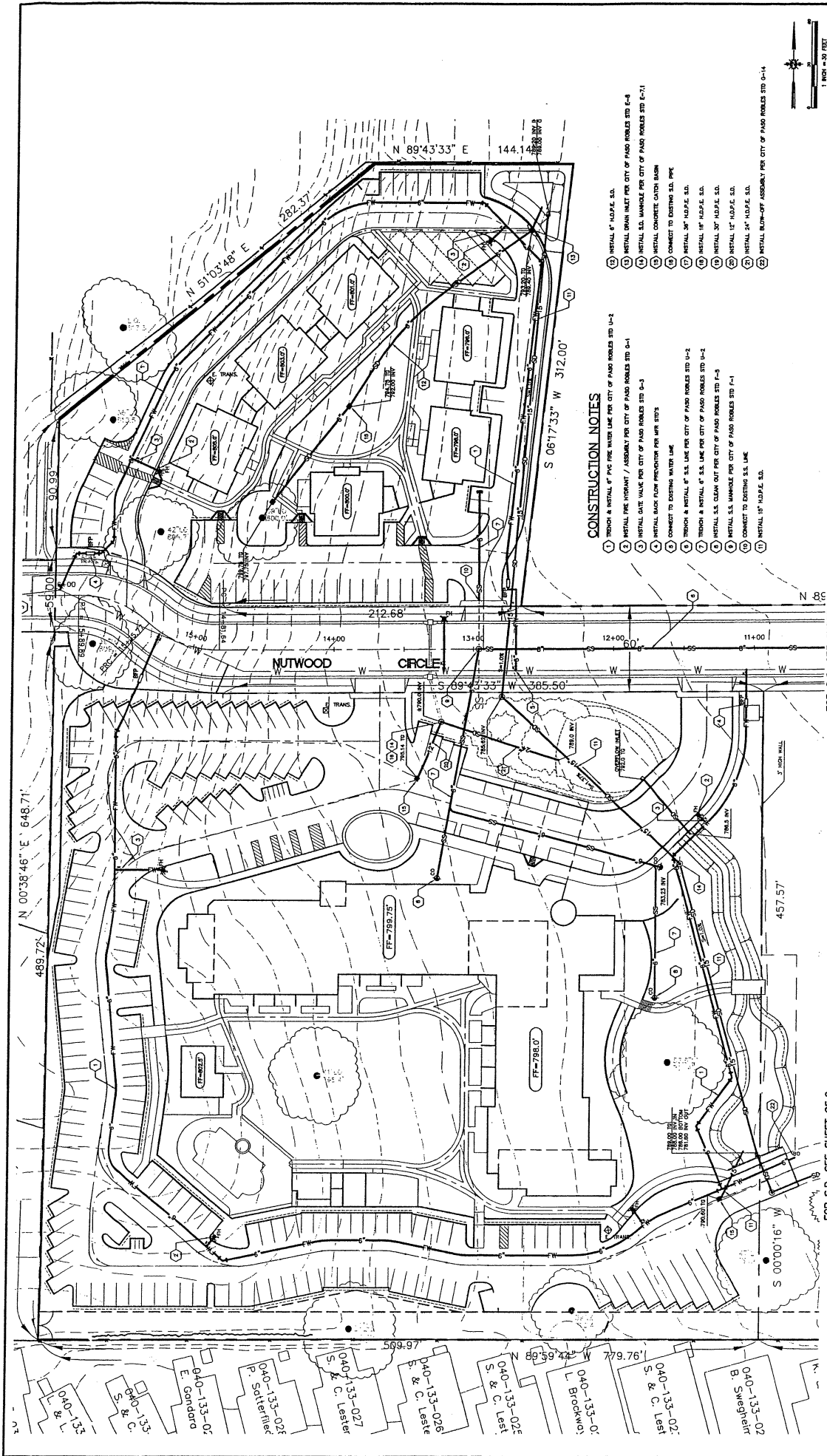
Nutwood Circle (Private)

Rancho Paso Drive

U.S. Highway 101 South

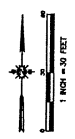
Graphic Scale  
1 inch = 40 ft.

Scale: 1" = 40'



**CONSTRUCTION NOTES**

- 1 TRENCH & INSTALL 6" PVC FIRE WATER LINE PER CITY OF PASO ROBLES STD U-2
- 2 INSTALL FIRE HYDRANT / ASSEMBLY PER CITY OF PASO ROBLES STD Q-1
- 3 INSTALL GATE VALVE PER CITY OF PASO ROBLES STD Q-3
- 4 INSTALL BACK FLOW PREVENTER PER ARI STD 5
- 5 CONNECT TO EXISTING WATER LINE
- 6 TRENCH & INSTALL 6" S.S. LINE PER CITY OF PASO ROBLES STD U-2
- 7 TRENCH & INSTALL 6" S.S. LINE PER CITY OF PASO ROBLES STD U-4
- 8 INSTALL S.S. CLEAN OUT PER CITY OF PASO ROBLES STD F-5
- 9 INSTALL S.S. MANHOLE PER CITY OF PASO ROBLES STD F-1
- 10 CONNECT TO EXISTING S.S. LINE
- 11 INSTALL 10" H.O.P.E. S.D.
- 12 INSTALL 6" H.O.P.E. S.D.
- 13 INSTALL DRAIN INLET PER CITY OF PASO ROBLES STD E-4
- 14 INSTALL S.D. MANHOLE PER CITY OF PASO ROBLES STD E-11
- 15 INSTALL CONCRETE CATCH BASIN
- 16 CONNECT TO EXISTING S.D. PIPE
- 17 INSTALL 30" H.O.P.E. S.D.
- 18 INSTALL 18" H.O.P.E. S.D.
- 19 INSTALL 30" H.O.P.E. S.D.
- 20 INSTALL 12" H.O.P.E. S.D.
- 21 INSTALL 24" H.O.P.E. S.D.
- 22 INSTALL BURN-OFF ASSEMBLY PER CITY OF PASO ROBLES STD Q-14



FOR S.D. SEE SHEET C5.0

FOR S.S. SEE SHEET C5.0

**RTIC**  
R THOMPSON CONSULTING, INC.  
CIVIL ENGINEERING

WEBB & MONSON HOTEL BY DURAND DESIGNS  
PRELIMINARY UTILITY PLAN  
CITY OF PASO DE ROBLES, CA

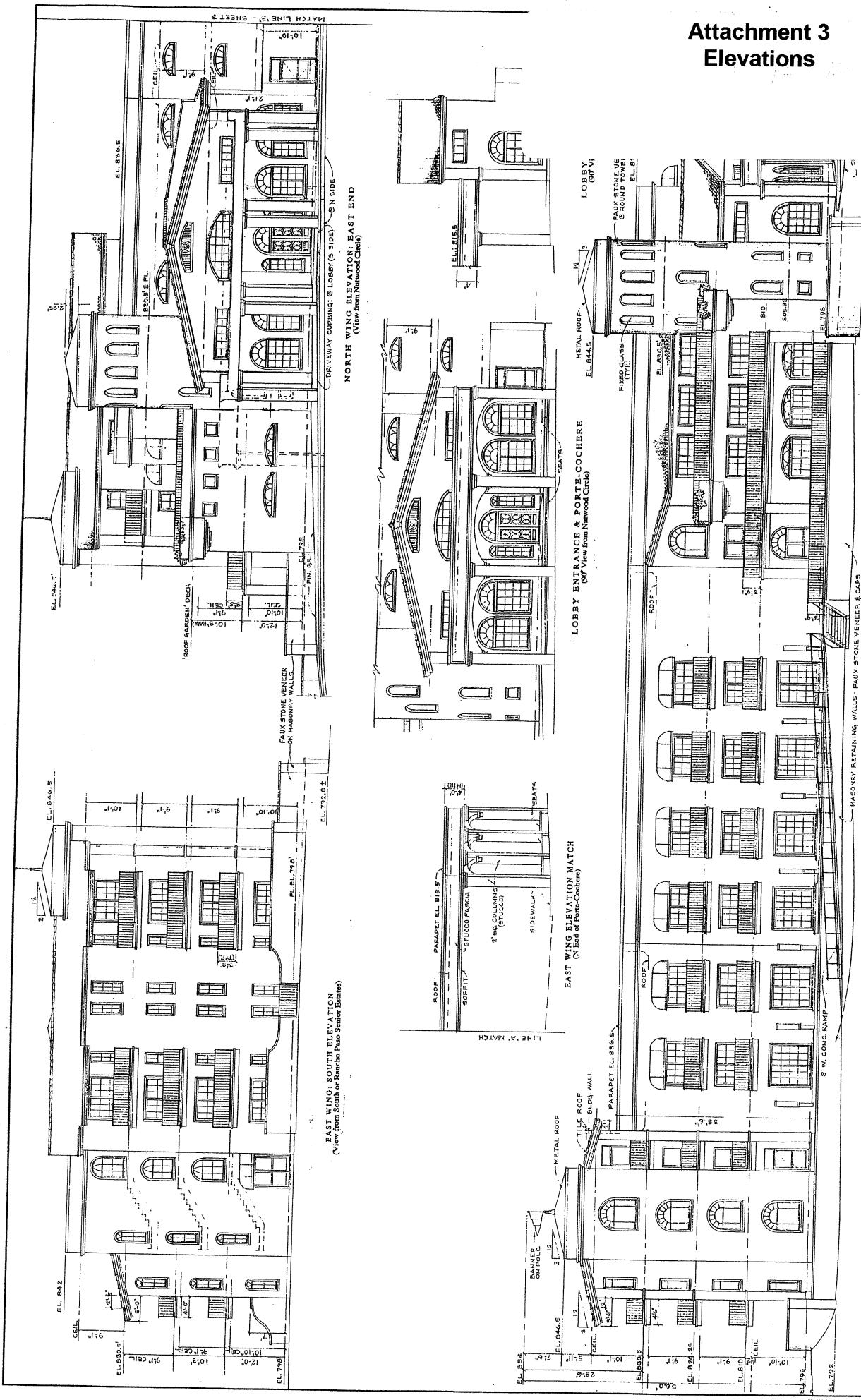
PROJECT: C40  
DRAWN BY: RUSSELL S. THOMPSON  
CHECKED BY: JAMES W. BROWN  
DATE: 7-5-05  
SCALE: JOB NUMBER: 7-5-05  
1" = 30'  
JOB NO.: 05-014  
SHEET NO.: 4 OF 5

REGISTERED PROFESSIONAL ENGINEER  
NO. 44137  
EXPIRES: 06-30-08  
RUSSELL S. THOMPSON  
CIVIL  
1000 MONTE PASO AVENUE  
PASO ROBLES, CA 93270  
(805) 441-1370

APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
CITY ENGINEER

JUL 05 2005  
YOUR SUPERVISOR

# Attachment 3 Elevations



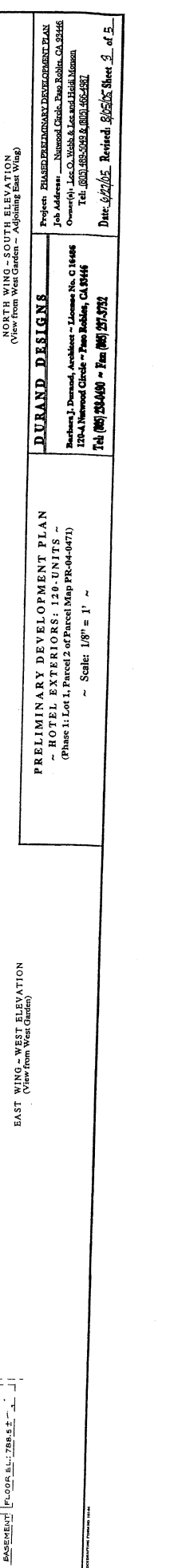
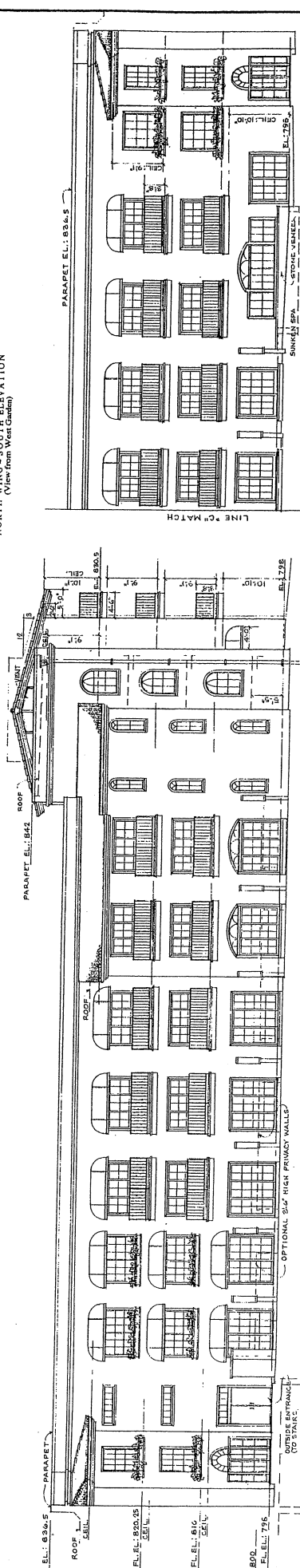
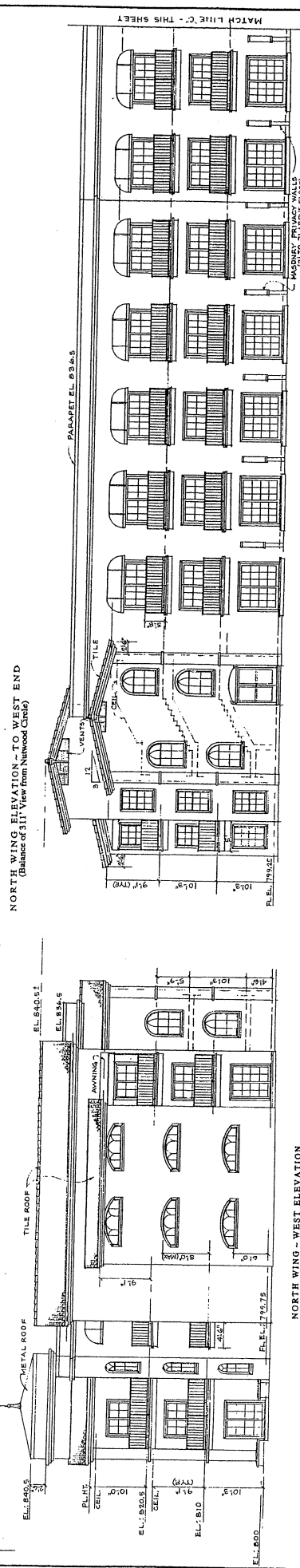
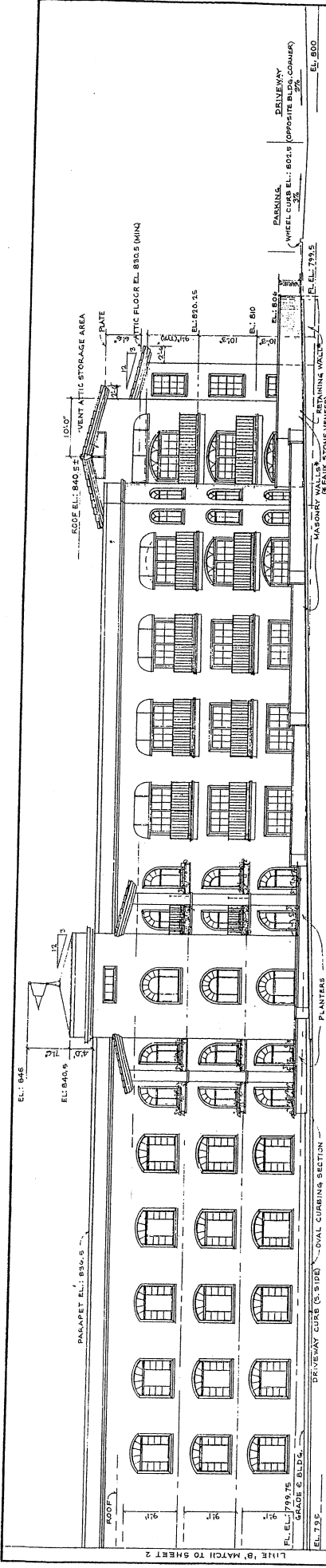
**DURAND DESIGNS**  
 Project: E24  
 Job Address:  
 Owner: (b) (4)  
 Barbara J. Durand, Architect - License No. C 16466  
 1244 Nutwood Circle - Paso Robles, CA 92546  
 Tel. (805) 238-6600 - Fax (805) 237-3752  
 Date: 4/25/02

**PRELIMINARY DEVELOPMENT PLAN**  
 ~ HOTEL EXTERIORS: 120 - UNITS ~  
 (Phase I: Lot 1, Parcel 2 of Parcel Map PR-04-0471)

Scale: 1/8" = 1'

EAST WING ELEVATION  
 (View from East Garden & Theatre Drive)



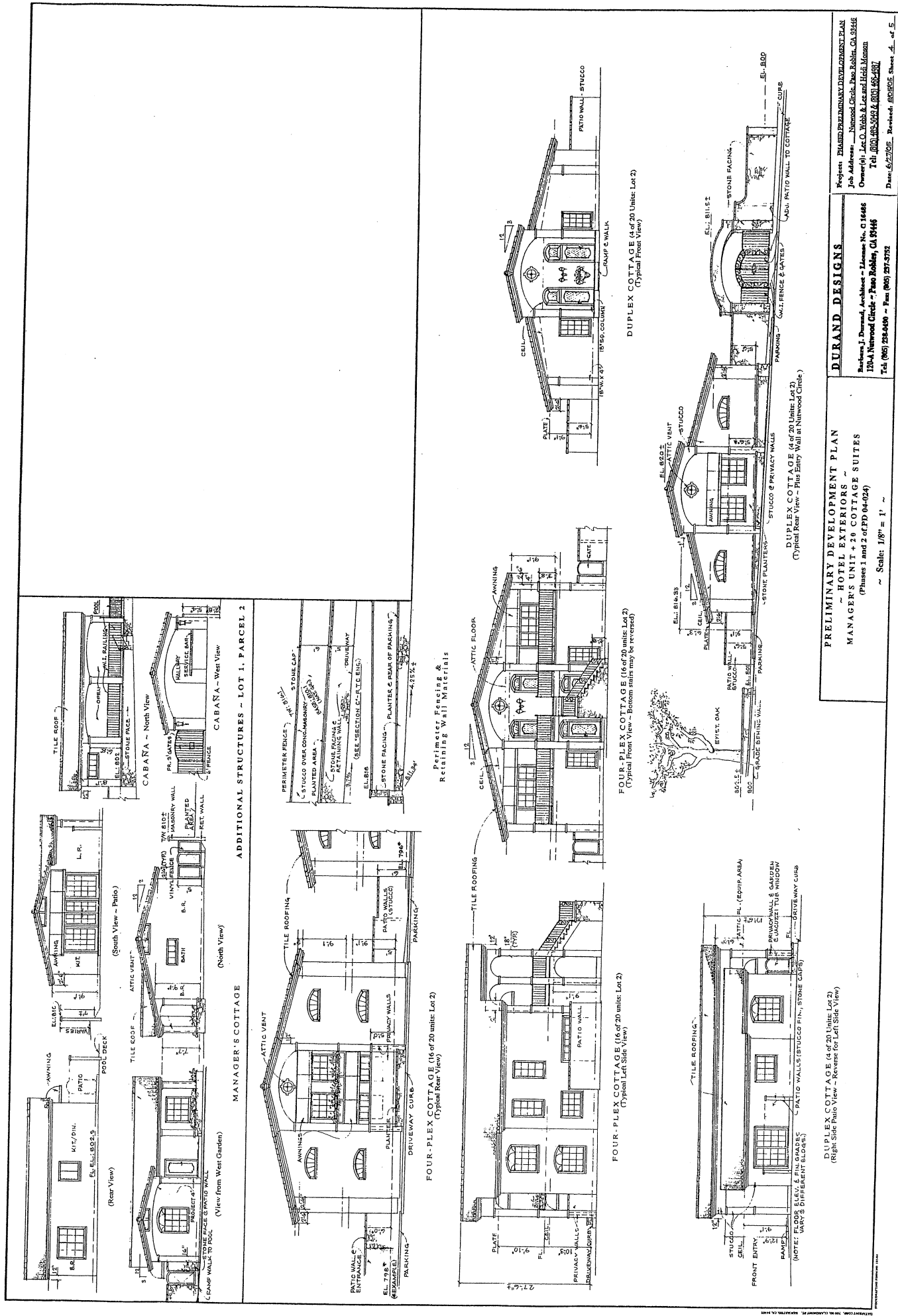


**PRELIMINARY DEVELOPMENT PLAN**  
 - HOTEL EXTERIORS: 120 - UNITS -  
 (Phase 1: Lot 1, Parcel 2 of Parcel Map PR-04-0471)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**

Project: PHASED PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Walnut Creek, Paso Robles, CA 92446  
 Owner: J. Durand, Architect - License No. C 16486  
 120-A Newwood Circle - Paso Robles, CA 92446  
 Tel: (805) 465-5093 & (805) 465-4387  
 Fax: (805) 465-4400 - Fax (805) 291-3732

Date: 6/27/05 Revised: 8/26/05 Sheet 3 of 5



**PRELIMINARY DEVELOPMENT PLAN**  
 ~ HOTEL EXTERIORS ~  
 MANAGER'S UNIT + 20 COTTAGE SUITES  
 (Phases 1 and 2 of PD 04-004)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**  
 Richard J. Durand, Architect - License No. C11648  
 1204 Network Circle - Palo Alto, CA 94406  
 Tel: (650) 254-0409 - Fax: (650) 257-3732

Project: PHASED PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Network Circle - Palo Alto, CA 94406  
 Owner(s): Joe O. Wirth & Associates/Herb Monson  
 Tel: (650) 485-5045 & (650) 482-4571  
 Date: 6/27/05 - Revised: 05/25/05 Sheet 4 of 5

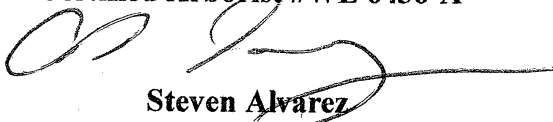
**A & T ARBOR**  
P.O. BOX 1311 TEMPLETON, CA 93465

**Tree Preservation Plan  
Addendum  
Durand Project  
Paso Robles, CA**

**8-01-05**

**Prepared by A & T Arborists  
and Vegetation Management**

**Chip Tamagni  
Certified Arborist #WE 6436-A**

  
**Steven Alvarez  
Certified Arborist #WE 511-A**

**RECEIVED**  
AUG 05 2005  
Community Development

Tract # \_\_\_\_\_

PD # 04-024

Building Permit # \_\_\_\_\_

**Project Description:** This project involves the construction of a hotel, cottages and accompanying parking areas located off theater drive in Paso Robles, CA. The area currently has nine valley oak trees (*Quercus lobata*) and none are planned for removal. There are two additional valley oaks located on the property adjacent to lot #2.

**Specific Mitigations Pertaining to the Project:** As outlined on the spreadsheet, all trees must have protective fencing in place and inspected by the arborists prior to grading. All impacts (grading for curbing, parking, retaining walls and footings) shall be monitored and any necessary root pruning, wetting, root fungicide application will be completed by the project arborists. The arborists shall be called a minimum of 48 hours before any needed monitoring. The arborists shall also be called out to monitor the retaining wall trenching near the neighboring trees near lot #2. The crz encroachment for tree #9 should not impact the tree long-term. The grade for this tree shall not be changed within the fencing nor shall any grading contribute to a ponding condition around the trunk.

The term “critical root zone” or CRZ is an imaginary circle around each tree. The radius of this circle (in feet) is equal to the diameter (in inches) of the tree. For example, a 10 inch diameter tree has a critical root zone with a ten foot radius from the tree. Working within the CRZ usually requires mitigations and/or monitoring by a certified arborist.

All trees potentially impacted by this project are numbered and identified on both the grading plan and the spreadsheet. Trees are numbered on the grading plans and in the field with an aluminum tag. Tree protection fencing is shown on the grading plan. In the field oak trees to be saved have yellow tape attached to the tag (no removals for this project). Both critical root zones and drip lines are outlined on the plans.

If pruning is necessary for building, road or driveway clearance, removal of limbs larger than 6 inches in diameter will require a city approved permit along with a deposit paid in advance (to the City of Paso Robles). The city will send out a representative to approve or deny the permit. Only 25% of the live crown may be removed. At this time, no trees appear to need any building clearance pruning.

### **Tree Rating System**

A rating system of 1-10 was used for visually establishing the general health and condition of each tree on the **spreadsheet**. The rating system is defined as follows:

<u>Rating</u>	<u>Condition</u>
0	Deceased
1	Evidence of massive past failures, extreme disease and is in severe decline.
2	May be saved with attention to class 4 pruning, insect/pest eradication and future monitoring.

- 3 Some past failures, some pests or structural defects that may be mitigated by class IV pruning.
- 4 May have had minor past failures, excessive deadwood or minor structural defects that can be mitigated with pruning.
- 5 Relatively healthy tree with little visual, structural and/or pest defects and problems.
- 6 Healthy tree that probably can be left in its natural state.
- 7-9 Has had proper arboricultural pruning and attention or have no apparent structural defects.
- 10 Specimen tree with perfect shape, structure and foliage in a protected setting (i.e. park, arboretum).

Aesthetic quality is defined as follows:

- **poor** - tree has little visual quality either due to severe suppression from other trees, past pruning practices, location or sparse foliage
- **fair** - visual quality has been jeopardized by utility pruning/obstructions or partial suppression and overall symmetry is average
- **good** - tree has good structure and symmetry either naturally or from prior pruning events and is located in an area that benefits from the trees position
- **excellent** - tree has great structure, symmetry and foliage and is located in a premier location. Tree is not over mature.

All trees on the site are relatively the same size and age (mature). Aesthetically speaking, the project has been designed to positively accent the trees. They all rate from good to excellent. The trees on site have a useful life expectancy of 50 to 80 years.

The following mitigation measures/methods must be fully understood and followed by anyone working within the critical root zone of any native tree. Any necessary clarification will be provided by us (the arborists) upon request.

1. It is the responsibility of the **owner or project manager** to provide a copy of this tree protection plan to any and all contractors and subcontractors that work within the critical root zone of any native tree and confirm they are trained in maintaining fencing, protecting root zones and conforming to all tree protection goals. It is highly recommended that each contractor sign and acknowledge this tree protection plan.
2. Any future changes (within the critical root zone) in the project will need Project Arborist review and implementation of potential mitigation measures before any said changes can proceed.
3. **Fencing:** The proposed fencing shall be shown in orange ink on the grading plan. It must be a minimum of 4' high chain link, snow or safety fence staked (with t posts 8 feet on center) at the edge of the critical root zone or line of encroachment for each tree or group of trees. The fence shall be up before any construction or earth moving begins. The owner shall be responsible for maintaining an erect fence throughout the construction period. The arborist(s), upon notification, will inspect the fence placement once it is erected. After this time, fencing shall not be moved without arborist inspection/approval. If the orange plastic fencing is used, a minimum of four zip ties

**18. Fertilization and Cultural Practices:** As the project moves toward completion, the arborist(s) may suggest either fertilization and/or mycorrhiza applications that will benefit tree health. Mycorrhiza offers several benefits to the host plant, including faster growth, improved nutrition, greater drought resistance, and protection from pathogens.

The included spreadsheet includes trees listed by number, species and multiple stems if applicable, scientific name, diameter and breast height (4.5'), condition (scale from poor to excellent), status (avoided, impacted, removed, exempt), percent of critical root zone impacted, mitigation required (fencing, root pruning, monitoring), construction impact (trenching, grading), recommended pruning and individual tree notes.

If all the above mitigation measures are followed, we feel there will be no long-term significant impacts to the native trees.

Please let us know if we can be of any future assistance to you for this project.

Steven G. Alvarez  
Certified Arborist #WC 0511

Chip Tamagni  
Certified Arborist #WE 6436-A

DURAND DESIGNS  
NUTWOOD CIRCLE  
HOTEL SITE

TREE PROTECTION SPREAD SHEET

1	2	3	4	5	6	7	8	9	10	11
TREE #	TREE SPECIES	TRUNK DBH	TREE CONDITION	CONST STATUS	DRIP-LINE % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	FIELD NOTES
1	0	0	0	0	0%	0	0	0	0	0
2	0	0	0	0	0%	0	0	0	0	0
3	VO	47"	8	I	50%	G,C	F,M	Y	C-4	MOVE VEH PKING, NAT PLTS
4	VO	53.5"	6	I	40%	G,C	F,M	Y	C-4	LAGOON ADJUSTMENT?
5	VO	39"	4	I	40%	G,C	F,M	Y	C-4	RD, PK, TRASH, PAVERS
6	VO	40"	4	I	30%	G,C	F,M	Y	C-4	PK, DEADWD PRUNING A MUST*
7	VO	41"	6	I	100%	G,C	F,M	Y	NONE	NO IRR, NO LAWN, NATIVE ONLY
8	VO	39"	4	I	15%	G,C	F,M	Y	NONE	PARKING, CURB
9	VO	39"	5	I	80%	G,C	F,M	Y	NONE	PK, CONST, GRADING
10	VO	42"	4	I	80%	G,C	F,M	Y	NONE	PK, CONST, GRADING
11	VO	36	3	I	20%	G,C	F,M	Y	NONE	PK, GRADING, CONST

→ 50% is landscape impact (drought tolerant)

- 1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
- 2 = TREE TYPE: COMMON NAME IE: W.O. = WHITE OAK
- 3 = TRUNK DIAMETER @ 4'
- 4 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
- 5 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
- 6 = DRIP-LINE: PERCENT OF IMPACTED DRIP-LINE

- 7 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
- 8 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTING PRUNING, PRUNING
- 9 = ARBORIST MONITORING REQUIRED: YES/NO
- 10 = PERSCRIBED PRUNING: CLASS 1-4
- 11 = FIELD NOTES

\*TREE # 6 HAS THREE LIMBS THAT ARE HAZARDIST AND OVER HANGING THE NEIGHBORING TRAILOR PARK THE LIMBS SHOULD BE REMOVED AS SOON AS POSSIBLE



**ASSOCIATED TRANSPC**

100 N. Hope Avenue, Suite 4, Santa Barbara, CA 93101

Richard L. Pool, P.E.  
Scott A. Schell, AICP

**Paso Robles**

**NOV 14 2006**

**Planning Division**

06113R01.WPD

November 13, 2006

John Falkenstien  
City of Paso Robles  
1000 Spring Street  
Paso Robles, CA 93446

***TRAFFIC IMPACT STUDY FOR THE DURAND PROJECT, CITY OF PASO ROBLES***

Associated Transportation Engineers (ATE) has prepared the following traffic study for the Durand Project, proposed in the southern portion of the City of Paso Robles. The project is proposing to construct a 120-room hotel on the southwest corner of the Theater Drive/Nutwood Circle intersection plus 20 hotel guest suites across the street on Nutwood Circle.

**EXISTING CONDITIONS**

Table 1 shows the Existing Weekday Midday, Weekday P.M., Friday P.M., and Saturday P.M. peak hour levels of service for the intersections comprising the Route 46 (W)/Route 101 interchange. Peak hour traffic volumes for the interchange were collected in April and June 2005. The counts include the number of trucks using the interchange.



**Table 1**  
**Route 46 (W)/Route 101 Interchange**  
**Existing Peak Hour Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	34.1/LOS C	35.6/LOS D	38.8/LOS D	36.4/LOS D
Rt 46(W)/Rt 101 NB	14.5/LOS B	14.6/LOS B	19.7/LOS B	17.7/LOS B
Rt 46(W)/Ramada	15.8/LOS C	15.5/LOS C	18.6/LOS B	14.2/LOS B

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection operates at LOS C during the Weekday Midday period and LOS D during the Weekday P.M., Friday P.M., and Saturday P.M. peak hour periods. The two intersections comprising the east side of the interchange operate at LOS B-C.

### NEAR-TERM CONDITIONS

Near-term traffic conditions were forecasted for the interchange assuming the additional traffic generated by the approved developments in the vicinity of the interchange, along with the near-term improvements planned for the interchange. The following section details the near-term level of service forecasting for the interchange.

### Approved Projects

There are several approved projects that will add traffic to the study-area street system in the near-term. Table 2 shows the approved projects in the area.

**Table 2**  
**Route 46 (W)/Route 101 Interchange - Approved Projects**

Project	Land Use	Size
Target Center <sup>a</sup>	Shopping Center	17,000 SF
La Vorgna Storage	Storage	2.8 Acres
McDonalds/Chevron	Fast-Food Restaurant Gas Station w/ Car Wash	3,152 SF 12 Pumps/1 Wash
Gheza Mini-Storage	Storage	3.75 Acres
Theater Drive Retail	Mixed Retail Self-Storage	39,650 SF 62,000 SF
Bellesara Suites Project	Lodging	62-Unit Hotel
1331 Vendels Circle - Benny Simmons	Light Industry	6,000 SF
1160 Ramada Drive - True Tube	Light Industry	6,900 SF
1375 Ramada Drive - Lavorgna	Light Industry	46,684 SF
1500 Ramada Drive - Pokrajac	Light Industrial/Warehouse	42,800 SF
The Inns at Vintner's Village Project	Lodging	138 rooms

<sup>a</sup> Remaining square footage under original shopping center approval.

### Route 46 (W)/Route 101 Improvements

The City commissioned a traffic study of the interchange in the Summer of 2002. That study analyzed traffic conditions at the interchange and included improvements to provide additional capacity at the interchange in the near term. The near-term improvements that are being constructed and are expected to be completed in late 2006-early 2007 include:

- 1) Modify Route 101 SB Off-Ramp. Lengthen the ramp storage lanes to provide 500-550 feet of storage. The off-ramp contains one left-turn lane and two right-turn lanes. The inside right-turn lane is designated for southbound Theater Drive and the outside lane is designated for Route 46 (W) & Vine Street.
- 2) Modify Curb Return on Northeast Corner of SB Off-Ramp. Modify the curb return to accommodate truck turns.

- 3) Signalize Route 46 (W)/Ramada. Install traffic signals at this location.
- 4) Signal Interconnect & Coordination. Provide for interconnection of all of the traffic signals in the interchange and provide a timing plan for signal coordination.

### Levels of Service

Table 3 shows the near-term level of service forecasts for the Route 46 (W)/Route 101 interchange assuming the near-term traffic volume projections for the Existing + Approved Projects scenario and the near-term improvements listed above.

**Table 3**  
**Route 46 (W)/Route 101 Interchange - Near-Term Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	35.8/LOS D	40.9/LOS D	57.7/LOS E	45.5/LOS D
Rt 46(W)/Rt 101 NB/Ramada	18.4/LOS B	19.9/LOS B	21.7/LOS C	21.6/LOS C

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB, and Rt 46(W)/Rt 101 NB and Rt 46(w)/Ramada are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection is forecast to operate at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period with the near-term developments and improvements. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

### PROJECT IMPACTS

#### Trip Generation

Trip generation estimates were calculated for the Durand Project based on ITE, Caltrans and SANDAG rates. Table 4 summarizes the trip generation calculations for the Weekday Midday, Weekday P.M., Friday P.M., and Saturday P.M. peak hour periods.

**Table 4  
Project Trip Generation**

Use	Size	Peak Hour Period	Trips
Hotel	120 Rooms 20 Suites	Weekday MIDDAY	134
		Weekday P.M.	83
		Friday P.M.	83
		Saturday P.M.	101

Trip generation rates derived from ITE, SANDAG & Caltrans studies.

The trips that will be generated by the project were distributed and assigned to the Route 46 (W)/Route 101 Interchange using the distribution percentages shown in Table 5. These percentages were developed from marketing data/traffic studies of other commercial projects in the area, as well as consideration of the street system and the surrounding residential and commercial centers. Most of the traffic is expected to/from Route 101 (65%) with the remainder via the other surface streets in the area. 12% of the traffic will be local and/or link to other uses in the Theater Drive area.

**Table 5  
Project Trip Distribution**

Origin/Destination	Direction	Percent
Route 101	North	35%
Route 101 <sup>a</sup>	South	30%
Vine Street	North	10%
SR 46 West	West	5%
Ramada Drive n/o SR 46 West	East	2%
Ramada Drive s/o SR 46 West	East	1%
Main Street <sup>b</sup>	South	5%
Local	--	12%
<b>Total</b>		<b>100%</b>

<sup>a</sup> Traffic to/from south on Route 101 assigned to the Main Street interchange.

<sup>b</sup> Traffic to/from south on Main Street assigned to the Main Street interchange.

**NEAR-TERM + PROJECT LEVELS OF SERVICE**

Table 6 shows the near-term + project level of service forecasts for the Route 46 (W)/Route 101 Interchange. The levels of service assume the traffic volume projections for the Existing + Approved + Project condition.

**Table 6  
Route 46 (W)/Route 101 Interchange  
Near-Term + Project Peak Hour Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	39.3/LOS D	41.9/LOS D	61.1/LOS E	46.4/LOS D
Rt 46(W)/Rt 101 NB/Ramada	17.8/LOS B	18.2/LOS B	21.8/LOS C	22.6/LOS C

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB, and Rt 46(W)/Rt 101 NB and Rt 46(w)/Ramada are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection is forecast to operate at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

**ROUTE 46 (W)/ROUTE 101 SB OFF-RAMP QUEUING**

The maximum queues forecasted on the Route 101 SB off-ramp will occur during the Friday and Saturday P.M. peak hour periods. With the additional traffic from the approved projects and the Durand Project, the maximum queues are forecasted at 405 feet during the Friday P.M. peak hour period and 435 feet during the Saturday P.M. peak hour period assuming the near-term improvements that are being constructed. The near-term improvements include lengthening the ramp to provide 550 feet of storage. Thus, the traffic on the southbound off-ramp could be accommodated with the additional storage provided by the near-term improvements.

## PROJECT SHARE OF NEAR-TERM IMPROVEMENTS

Table 7 shows the percentage contribution of traffic at the Route 46 (W)/Route 101 interchange for each of the approved projects that would contribute to the improvements, including the Durand Project.

**Table 7**  
**Route 46 (W)/Route 101 Interchange**  
**Traffic Contributions from Near-Term Projects**

Project	Traffic Contribution	% Share
McDonalds/Chevron	214 PHT	39.3%
Gheza Mini-Storage	15 PHT	2.8%
Theater Drive Retail/Storage	71 PHT	13.0%
Bellesara Suites	34 PHT	6.2%
1331 Vendels Circle Project - Benny Simmons	6 PHT	1.1%
1160 Ramada Drive Project - True Tube	7 PHT	1.3%
1375 Ramada Drive - Lavorgna	46 PHT	8.4%
1500 Ramada Drive - Pokrajac	30 PHT	5.5%
Inns at Vintner's Village Project	77 PHT	14.1%
Durand Project	45 PHT	8.3%
<b>Total</b>	<b>545 PHT</b>	<b>100%</b>

Traffic Contribution based on weekday peak hour trips (PHT) entering the interchange.

The traffic generated by the Durand Project would be 8.3% of the traffic that will be added by the approved projects.

**SUMMARY**

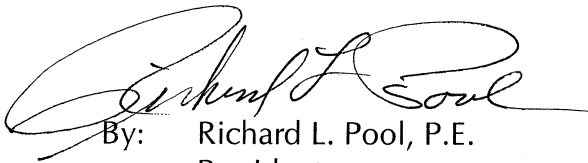
The projected traffic from the existing plus approved projects will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday

P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

The addition of the projected Durand Project traffic will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

The Durand Project's portion of the traffic that will be added to the interchange by the approved project is 8.3%.

Associated Transportation Engineers



By: Richard L. Pool, P.E.  
President

RLP/DLD/LDH

attachments



## MEMORANDUM

**TO:** Susan DeCarli  
**FROM:** John Falkenstien  
**SUBJECT:** PD 04-024 Durand  
**DATE:** February 27, 2007

### Traffic

This application could not be processed at the time of its completion in the fall of 2005 because the traffic it would generate would reduce the level of service of the Highway 46W-101 intersection below D, and therefore fall out of compliance with general plan conformance. This determination was based on the cumulative traffic generated by a queue of previously approved projects.

In 2006, the City began the design and environmental process for ultimate interchange improvements. Through this process certain properties were identified as being needed to accomplish the improvements. The City subsequently purchased a number of properties over the course of the last year. Those properties held entitlements for projects in the queue. Also during the last year, interim improvements have been constructed on the southbound 101 off ramp to increase interchange capacity.

With the removal of projects on properties purchased by the City and the installation of interim intersection improvements considered, an updated traffic analysis was done for the Durand project. With the exception of the Friday peak hour, the intersection will operate in general plan conformity in the near term. Durand will mitigate their proportionate impact on the interchange by participation in the cost of the ultimate interchange improvements.

### Streets

The subject property takes access from Nutwood Circle, a private street with connection to Theatre Drive. The City Council adopted a plan line for Theatre Drive in April, 2004. The applicant will be required to construct improvements on Theatre Drive in accordance with the adopted plan line.

Nutwood Circle was created with Tract 1699 in the County. Nutwood Circle was offered for dedication on the final tract map but the offer was rejected by the County Board of Supervisors. Since all of Nutwood Circle within the City is surrounded by commercial property under one ownership, it is recommended that Nutwood Circle remain a private street.

As a private street, the design of Nutwood Circle should be incorporated into the site and landscape plan for the entire project. Pedestrian paths with decorative paver blocks and attractive streetscape features should be considered.



## **Sewer and Water**

Water is available to the project from 10-inch and 16-inch water lines in Theatre Drive. Both lines terminate at the south boundary of the Boatman's furniture building. The 10-inch water main will need to be extended to the south boundary of the project property to provide domestic and fire service to the project. Water meters and the double check valve assembly for the fire line will be placed on the Theatre Drive frontage. Fire hydrants will be required along Theatre Drive at 300-foot intervals. Fire hydrants must be placed on-site in accordance with plans approved by Emergency Services.

Sanitary sewer is available to the project from a 10-inch line in Theatre Drive that terminates at the south boundary of the Boatman's furniture building. The project will be served with one private sewer line connection in Theatre Drive.

## **Recommended Site Specific Conditions**

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1. City Standard street improvements shall be constructed on Theatre Drive along the frontage of the project in accordance with the plan line for Theatre Drive adopted by the City Council and plans approved by the City Engineer. An appropriate paving transition shall be extended southerly in accordance with plans approved by the City Engineer. Drainage structures must be designed and constructed as needed.
2. The existing overhead utilities along the east side of Theatre Drive across from the frontage of the project shall be relocated underground.
3. The 10-inch water main in Theatre Drive shall be extended to the south boundary of the project. Domestic lines serving the project shall be metered on the Theatre Drive frontage. The fire line/s double check valve assembly shall be placed on the Theatre Drive frontage in an underground vault or otherwise screened as approved by the City Planner.
4. The 10-inch sewer line in Theatre shall be extended southerly as needed to provide for a private sewer line to be extended from Theatre Drive to serve the project.
5. Storm water detention facilities shall be placed on-site in accordance with City Standards to mitigate the impact of increased volumes of storm water due to development of the site. Storm water quality devices shall be installed in accordance with a Storm Water Pollution Prevention Plan and plans approved by the City Engineer.
6. The applicant shall enter into an agreement to pay a proportionate share of the cost of Highway 46W-101 interchange improvements, as determined by the City, and to participate in the formation of an assessment district for those improvements.



**RESOLUTION NO:**

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PASO ROBLES  
ADOPTING NEGATIVE DECLARATION FOR  
PLANNED DEVELOPMENT 04-024 FOR THE DURRAND HOTEL AND SUITES  
2805 THEATRE DRIVE (ACCESSED FROM NUTWOOD CIRCLE)  
APPLICANTS - LEE MONSON AND LEE WEBB  
(APNs: 009-851-016 AND -023)**

**WHEREAS**, Planned Development 04-024 has been filed by Lee Monson and Lee Webb; and

**WHEREAS**, Planned Development 04-024 is a proposal to construct a 120 s.f. hotel and 20 guest cottage suites; and

**WHEREAS**, the General Plan land use designation is RC (Regional Commercial) and the Zoning District is Highway Commercial – Planned Development (C2-PD), and

**WHEREAS**, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City’s Procedures for Implementing CEQA, an Initial Study and a Draft Negative Declaration was prepared and circulated for public review and comment; and

**WHEREAS**, no public comments or responses were received in regard to the Draft Negative Declaration and Initial Study, which is included in Exhibit A; and

**WHEREAS**, Public Notice of the proposed Negative Declaration was posted as required by Section 21092 of the Public Resources Code; and

**WHEREAS**, a public hearing was conducted by the Planning Commission on February 27, 2007 to consider the Initial Study, the proposed Negative Declaration prepared for the proposed project, and to accept public testimony on the Planned Development and environmental determination; and

**WHEREAS**, based on the information and analysis contained in the Initial Study prepared for this project and testimony received as a result of the public notice, the Planning Commission finds that there is no substantial evidence that there would be a significant impact on the environment as a result of the development and operation of the proposed project. This finding is based on the Mitigation Monitoring Program included in the General Plan Environmental Impact Report.

**NOW, THEREFORE, BE IT RESOLVED**, by the Planning Commission of the City of El Paso de Robles, based on its independent judgment, that it does hereby adopt a Negative Declaration for Planned Development 04-024 in accordance with the Statutes and Guidelines of the California Environmental Quality Act (CEQA) and the City’s Procedures for Implementing CEQA.

**PASSED AND ADOPTED** THIS 27<sup>th</sup> day of February, 2007 by the following roll call vote:

AYES: Commissioners –  
NOES: Commissioners –  
ABSENT: Commissioner -  
ABSTAIN: Commissioners -

---

CHAIRMAN MARGARET HOLSTINE

ATTEST:

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RON WHISENAND, PLANNING COMMISSION SECRETARY

# CITY OF PASO ROBLES – PLANNING DIVISION INITIAL STUDY

## 1. GENERAL PROJECT INFORMATION

**PROJECT TITLE:** Durand Hotel and Cottages - PD 04-024

**LEAD AGENCY:** City of Paso Robles - 1000 Spring Street, Paso Robles, CA 93446

**Contact:** Susan DeCarli, AICP, City Planner  
**Telephone:** (805) 237-3970

**PROJECT LOCATION:** 2805 Theatre Drive (APN 009-851-023 and -016)

**PROJECT PROPONENT:** Applicant: Lee Monson and Lee Webb  
10519 Colorado Road, Atascadero, CA 93422

**LEAD AGENCY CONTACT/  
INITIAL STUDY PREPARED BY:** Susan DeCarli, AICP, City Planner

**Telephone:** (805) 237-3970  
**Facsimile:** (805) 237-3904  
**E-Mail:** sdecarli@prcity.com

**GENERAL PLAN DESIGNATION:** Regional Commercial (RC)

**ZONING:** Highway Commercial – Planned Development (C2-PD)

## 2. PROJECT DESCRIPTION

The proposed project is a request for a Planned Development application for a 120 room resort hotel and 20 cottages. There are two properties involved with this project, which are accessed from a private street, Nutwood Circle.

Surrounding land uses: Land uses to the north include commercial retail and service uses. Properties to the west include single family residences. A senior mobile home park is located to the south of the hotel site. Property located to the east is used as commercial retail on the north side of Nutwood Circle, and is there is a temporary use of mobile home sales on the south side of Nutwood Circle. The properties proposed for development are currently undeveloped.

The 120 unit hotel is proposed on the south side of Nutwood Circle, and the 20 cottages are proposed across the street to the north. A loop driveway is proposed on both properties for site access and circulation, and for the parking lots. The hotel site includes a central open space area, pool and other on-site amenities. The hotel is proposed to include meeting rooms, guest breakfast services, accessory gift shops and a wine tour service desk. An on-site caretakers unit is proposed on the hotel site. The cottages are intended to accommodate guests that would prefer an extended stay, therefore the cottages are proposed to be designed as suites.

There are several oak trees located on the hotel site, which are proposed to be protected within the development. An Arborist Report was prepared for this project, which includes tree protection measures. Most drainage that would result from the project is designed to be directed into landscape areas and water features. The applicant will be required to extend utility lines for City water and sewer services from the project sites to City utility lines in Theatre Drive. A traffic impact study was prepared for this project. The study evaluated project trip generation and impacts on surrounding circulation facilities, including the interchange of Highways 101 and 46 West. Since this project is proposed within the impact area of the interchange, the applicant will be required to participate in the interchange assessment district established for projects in the impact area. Further discussion of traffic impacts and mitigation measures is provided in the Transportation/Circulation Analysis of this Initial Study.

**3. OTHER AGENCIES WHOSE APPROVAL MAY BE REQUIRED (For example, issuance of permits, financing approval, or participation agreement):**

None.

**4. EARLIER ENVIRONMENTAL ANALYSIS AND RELATED ENVIRONMENTAL DOCUMENTATION:**

This Initial Study incorporates by reference the City of El Paso de Robles General Plan Environmental Impact Report (EIR) (SCH#2003011123).

**5. CONTEXT OF ENVIRONMENTAL ANALYSIS FOR THE PROJECT:**

This Initial Study relies on expert opinion supported by the facts, technical studies, and technical appendices of the City of El Paso de Robles General Plan EIR. These documents are incorporated herein by reference. They provide substantial evidence to document the basis upon which the City has arrived at its environmental determination regarding various resources.

**6. PURPOSES OF AN INITIAL STUDY**

The purposes of an Initial Study for a Development Project Application are:

- A. To provide the City with sufficient information and analysis to use as the basis for deciding whether to prepare an Environmental Impact Report, a Mitigated Negative Declaration, or a Negative Declaration for a site specific development project proposal;
- B. To enable the Applicant of a site specific development project proposal or the City as the lead agency to modify a project, mitigating adverse impacts before an Environmental Impact Report is required to be prepared, thereby enabling the proposed Project to qualify for issuance of a Negative Declaration or a Mitigated Negative Declaration;
- C. To facilitate environmental assessment early in the design of a project;
- D. To eliminate unnecessary EIRs;
- E. To explain the reasons for determining that potentially significant effects would not be significant;
- F. To determine if a previously prepared EIR could be used for the project;
- G. To assist in the preparation of an Environmental Impact Report if one is required; and

H. To provide documentation of the factual basis for the finding of no significant effect as set forth in a Negative Declaration or a Mitigated Negative Declaration prepared for the a project.

## **7. EXPLANATION OF ANSWERS FOUND ON THE ENVIRONMENTAL CHECKLIST FORM**

### **A. Scope of Environmental Review**

This Initial Study evaluates potential impacts identified in the following checklist.

### **B. Evaluation of Environmental Impacts**

1. A brief explanation is required for all answers to the questions presented on the following Environmental Checklist Form, except where the answer is that the proposed project will have “No Impact.” The “No Impact” answers are to be adequately supported by the information sources cited in the parentheses following each question or as otherwise explained in the introductory remarks. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors and/or general standards. The basis for the “No Impact” answers on the following Environmental Checklist Form is explained in further detail in this Initial Study in Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 10 (Context of Environmental Analysis for the Project).
2. All answers on the following Environmental Checklist Form must take into account the whole action involved with the project, including implementation. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more “Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.
4. Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level. Mitigation Measures from Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). See Section 4 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 11 (Earlier Analysis and Background Materials) of this Initial Study.
6. References to the information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the Environmental Checklist Form. See Section 11 (Earlier Analysis and Related Environmental Documentation). Other sources used or individuals contacted are cited where appropriate.
7. The following Environmental Checklist Form generally is the same as the one contained in Title 14, California Code of Regulations; with some modifications to reflect the City’s needs and requirements.
8. Standard Conditions of Approval: The City imposes standard conditions of approval on Projects. These conditions are considered to be components of and/or modifications to the Project and some reduce or

minimize environmental impacts to a level of insignificance. Because they are considered part of the Project, they have not been identified as mitigation measures. For the readers' information, the standard conditions identified in this Initial Study are available for review at the Community Development Department.

9. Certification Statement: The statements made in this Initial Study and those made in the documents referenced herein present the data and information that are required to satisfy the provisions of the California Environmental Quality Act (CEQA) – Statutes and Guidelines, as well as the City's Procedures for Implementing CEQA. Further, the facts, statements, information, and analysis presented are true and correct in accordance with standard business practices of qualified professionals with expertise in the development review process, including building, planning, and engineering.



**8. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The proposed project may potentially affect the environmental factors checked below, and may involve at least one impact that is a “Potentially Significant Impact” or is “Potentially Significant Unless Mitigated,” if so indicated on the following Environmental Checklist Form (Pages 8 to.15)

- |   |   |  |
|---|---|--|
| <input type="checkbox"/> Land Use & Planning  | <input type="checkbox"/> Transportation/Circulation         | <input type="checkbox"/> Public Services             |
| <input type="checkbox"/> Population & Housing | <input type="checkbox"/> Biological Resources               | <input type="checkbox"/> Utilities & Service Systems |
| <input type="checkbox"/> Geological Problems  | <input type="checkbox"/> Energy & Mineral Resources         | <input type="checkbox"/> Aesthetics                  |
| <input type="checkbox"/> Water                | <input type="checkbox"/> Hazards                            | <input type="checkbox"/> Cultural Resources          |
| <input type="checkbox"/> Air Quality          | <input type="checkbox"/> Noise                              | <input type="checkbox"/> Recreation                  |
|   | <input type="checkbox"/> Mandatory Findings of Significance |  |

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**9. ENVIRONMENTAL DETERMINATION:** On the basis of this initial evaluation: I find that:

The proposed project could not have a significant effect on the environment; and, therefore, a **NEGATIVE DECLARATION** will be prepared.

Although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. Therefore, a **MITIGATED NEGATIVE DECLARATION** will be prepared.

The proposed project may have a significant effect on the environment; and, therefore an **ENVIRONMENTAL IMPACT REPORT** is required.

The proposed project may have a significant effect(s) on the environment, but one or more effects (1) have been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) have been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a “potentially significant impact” or is “potentially significant unless mitigated.”

Therefore, an **ENVIRONMENTAL IMPACT REPORT** is required, but it will analyze only the effect or effects that remain to be addressed.

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

February 7, 2007

\_\_\_\_\_  
Susan DeCarli, AICP, City Planner

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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**I. LAND USE AND PLANNING.** Would the Proposal:

- a) Conflict with general plan designation or zoning?  
(Sources: 1 & 8)

*Discussion: The proposed project is consistent with the intent of the Regional Commercial land use designation of the General Plan and the Highway Commercial zoning district since it is a proposal for a regionally serving tourist destination hotel development.*

- b) Conflict with applicable environmental plans or policies adopted by agencies with jurisdiction over the project?  
(Sources: 1 & 3)

*Discussion: The proposed project complies with the EIR recently certified for the City General Plan Update, 2003 and there are no other agencies with jurisdiction over this project.*

- c) Be incompatible with existing land uses in the vicinity?  
(Sources: 1 & 3)

*Discussion: The surrounding uses include a mix of commercial and residential uses. The proposed hotel and cottage uses would provide an appropriate transition from the existing residential uses to the commercial uses. Dense landscaping along the southern property line should be incorporated into the hotel project and western property line for the cottages, to provide additional buffering between the hotel uses and adjacent residences. Also, as required in the Zoning Code, all outdoor lighting will need to be shielded and downcast to reduce light exposure to adjacent properties from the proposed project. The building footprint of the hotel are proposed to be setback at least 100 feet from the southern property line, which will reduce lighting impacts to surrounding properties.*

- d) Affect agricultural resources or operations (e.g., impacts to soils or farmlands, or impacts from incompatible uses)?

*Discussion: The project site is an urban infill property with no agricultural uses, resources or operations on the property.*

- e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)?  
(Sources: 1 & 3)

*Discussion: The project is proposed on an infill lot and will not disrupt or divide the established community.*

**II. POPULATION AND HOUSING.** Would the proposal:

- a) Cumulatively exceed official regional or local population projections? (Sources: 1 & 3)

*Discussion: The proposed project is consistent with the build-out alternative planned for and evaluated in the General Plan Update, 2003 and EIR. It is not large enough to result in creating a significant cumulative population growth impacts.*

## 10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- b) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? (Sources: 1 & 3)
- 

*Discussion: This is an existing infill property. The project will not extend major infrastructure that would induce substantial growth since there are existing services and infrastructure surrounding the property to serve the proposed project.*

- c) Displace existing housing, especially affordable housing? (Sources: 1, 3, & 5)
- 

*Discussion: The properties are currently vacant, therefore, the project will not displace existing housing.*

### III. GEOLOGIC PROBLEMS. Would the proposal result in or expose people to potential impacts involving:

- a) Fault rupture? (Sources: 1, 2, & 3)
- 

*Discussion: The potential for and mitigation of impacts that may result from fault rupture in the project area are identified and addressed in the General Plan EIR, pg. 4.5-8. There are two known fault zones on either side of this valley. The Rinconada Fault system runs on the west side of the valley. The San Andreas Fault is on the east side of the valley and runs through the community of Parkfield east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the Uniform Building Code to all new development within the City. Review of available information and examinations indicate that neither of these faults is active with respect to ground rupture in Paso Robles. Soils reports and structural engineering in accordance with local seismic influences would be applied in conjunction with any new development proposal. Based on standard conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant. In addition, per requirements of the Alquist-Priolo Earthquake Fault Zones, only structures for human habitation need to be setback a minimum of 50 feet of a known active trace fault.*

- b) Seismic ground shaking? (Sources: 1, 2, & 3)
- 

*Discussion: The City is located within an active earthquake area that could experience seismic ground shaking from the Rinconada and San Andreas Faults. The proposed structure will be constructed to current UBC codes. The General Plan EIR identified impacts resulting from ground shaking as less than significant and provided mitigation measures that will be incorporated into the design of this project including adequate structural design and not constructing over active or potentially active faults.*

- c) Seismic ground failure, including liquefaction? (Sources: 1, 2 & 3)
- 

*Discussion: Per the General Plan EIR, the project site is located in an area with soil conditions that have a potential for liquefaction or other type of ground failure due to seismic events due to soil conditions. The EIR identifies measures to reduce this potential impact, which will be incorporated into this project. This includes a requirement to conduct a site-specific analysis of liquefaction potential. Based on analysis results, the project design and construction will include specific design requirements to reduce the potential impacts on structures due to liquefaction to a less than significant level.*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
d) Seiche, tsunami, or volcanic hazard? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Landslides or Mudflows? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: d. and e. The project site is not located near bodies of water or volcanic hazards, nor is the site located in an area subject to landslides or mudflows.</i>				
f) Erosion, changes in topography or unstable soil conditions from excavation, grading, or fill? (Sources: 1, 2, 3, & 4)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: Per the General Plan EIR the soil condition is not erosive or otherwise unstable. As such, no significant impacts are anticipated. The site is relatively flat and will need minimal grading.</i>				
g) Subsidence of the land? (Sources: 1, 2, & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: See Item c.</i>				
h) Expansive soils? (Sources: 4)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<i>Discussion: Per the General Plan EIR, Paso Robles is an area that has moderately expansive soils. This issue will be addressed through implementation of appropriate soil preparation as determined necessary by recommendations of site specific soils report. Therefore, impacts related to expansive soils will be less than significant.</i>				
i) Unique geologic or physical features? (Sources: 1 & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: There are no unique geologic or physical features on or near the project site.</i>				

**IV. WATER.** Would the proposal result in:

a) Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff? (Sources: 1, 3, & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<i>Discussion: The project sites are currently vacant, however, the project hardscape and landscaping features are designed to retain all surface drainage on site to minimize surface drainage runoff. Given the drainage design for the site, the project will only slightly increase the amount of surface runoff and decrease absorption rate, which will be less than significant.</i>				
b) Exposure of people or property to water related hazards such as flooding? (Sources: 1, 3, & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Discussion: There is no potential to expose people or property to water related hazards due to this project since it is not in a flood zone.*

## 10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>c) Discharge into surface waters or other alteration of surface water quality (e.g., temperature, dissolved oxygen or turbidity)? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: Water discharge from this project will ultimately be conveyed after short term detention, into the Salinas River. Water from the site will be required to be filtered through NPDES compliant and approved system to remove contaminants prior to discharge into surface water in compliance with the Urban Water Management Plan, such as the Salinas River, so that it does not result in degraded water quality.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>d) Changes in the amount of surface water in any water body? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: There is no water body on or near the project site.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e) Changes in currents, or the course or direction of water movement? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: This project could not result in changes in currents or water movement since there is no water course in the vicinity that could be affected by this project.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>f) Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of groundwater recharge capability? (Sources: 1,3, &amp; 7)</p> <p><i>Discussion: The proposed project does not directly withdraw water resources. The project is consistent with the build-out scenario in the General Plan and planned water use and reserve capacity.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>g) Altered direction or rate of flow of groundwater? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: This project could not result in alterations to the direction or rate of groundwater flow since this project does not directly extract groundwater or otherwise significantly affect these resources.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>h) Impacts to groundwater quality? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: The project will not affect groundwater quality since this project does not directly extract groundwater or otherwise affect these resources, and the proposed uses do not utilize materials or methods that would result in reduced groundwater quality. This project will not change existing water quality from discharging in surface waters with implementation of standard storm water discharge infrastructure that is in compliance with the National Pollution Discharge Elimination System (NPDES) requirements.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>i) Substantial reduction in the amount of groundwater otherwise available for public water supplies? (Sources: 1, 3, &amp; 7)</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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**10 Environmental Checklist Form**

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
ISSUES (and Supporting Information Sources):				

*Discussion: Refer to response f.*

**V. AIR QUALITY.** Would the proposal:

- a) Violate any air quality standard or contribute to an existing or projected air quality violation? (Sources: 1, 3, & 7)

*Discussion: As an infill development site, in compliance with planned development for this zoning district previously anticipated and evaluated in the General Plan EIR, the project will not violate air quality standards.*

- b) Expose sensitive receptors to pollutants? (Sources: 1, 3, & 7)

*Discussion: There are no sensitive receptors such as schools, hospitals, etc. within the near vicinity that could be impacted by this project.*

- c) Alter air movement, moisture, or temperature? (Sources: 1, 3, & 7)

*Discussion: This project does not have the potential to significantly alter air movement, moisture, or temperature since the project is a small scale infill project.*

- d) Create objectionable odors?

*Discussion: Given the nature of the proposed uses, this project does not generally have the potential to create objectionable odors.*

**VI. TRANSPORTATION/CIRCULATION.** Would the proposal result in:

- a) Increased vehicle trips or traffic congestion? (Sources: 1, 3, & 7)

*Discussion: A Traffic Impact Study was prepared by Associated Transportation Engineers, November 2006. The study assessed the existing conditions, proposed project trip generation, and evaluated potential traffic impacts that may result from the project for the near-term with approved projects in the impact area. The study also evaluated impacts that may result from the project in the near-term with interim improvements. Additionally, the study evaluated impacts that may result from the proposed project in regard to the SB 46W/101 off-ramp queuing. It also assigned traffic contribution percentages (see Table 7) for approved projects (including the Durand Hotel project) for the purpose of assessing project cost for individual projects for the near-term improvements. See Attachment 2, Traffic Impact Study.*

*Background:  
When this project was originally proposed (2 years ago), it was determined that the traffic impacts that may result from this project with other projects already approved in the vicinity, would exceed the adopted Level of Service (LOS) thresholds for the Hwy.101/46W interchange for both the City and Caltrans. Since then, interim improvements to the interchange including extending the S/B 101 offramp and other improvements (i.e. traffic signals, turn lanes, etc.) have been constructed. This specific improvement addresses safety issues of freeway queuing. Additionally, several projects that were previously entitled by the City have been eliminated from the approved projects list, since the City has*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact
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*acquired these properties for future long-term interchange improvements. However, since then regional traffic has increased, so that even with the properties/projects removed from the approved project list (and the associated trips generated that would impact the interchange), regional traffic has increased. Therefore, the project plus the current existing conditions and entitled projects, still exceeds the LOS for various weekday, Friday and Saturday peak hours for vehicle delays.*

*The General Plan and EIR identify interim and long-term improvements for the interchange. The City and Caltrans have completed the interim improvements, which has reduced safety queues and delays at the interchange. Additionally, the approved projects list has been shortened because the City has been implementing the long-term mitigation measures, and has been purchasing properties (with approved projects), thus taking trips out of the list of cumulative projects, for the purpose of using these properties for the long-term capacity increasing mitigation measure improvements. Additionally, the city is implementing long-term mitigation measures by assembling the Impact Assessment District which applies to all properties entitled within the interchange impact area, to participate in paying the projects per rata share of impacts.*

*Therefore, the City is in the process of mitigating impacts associated with development in the Hwy. 101/46W interchange. When the long-term improvements are completed (anticipated to be complete in 2009), the LOS at the interchange is projected to be within acceptable limitations of the adopted General Plan. Therefore, with mitigation measures incorporated (participation in the interchange assessment district) and upon implementation of improvements at the interchange by the city and Caltrans, this project will ultimately result in less than significant traffic impacts.*

- b) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (Sources: 1, 3, & 7)

*Discussion: The proposed project does not include road improvements that may result in safety hazards or in incompatible uses.*

- c) Inadequate emergency access or inadequate access to nearby uses? (Sources: 1, 3, & 7)

*Discussion: The project is adequately served for emergency services.*

- d) Insufficient parking capacity on-site or off-site? (Sources: 1, 3, 7, & 8)

*Discussion: Per the Zoning Ordinance parking requirements this project requires 155 parking spaces for the hotel use and 30 spaces for the cottages. The applicant has proposed 176 spaces for the hotel, and 48 spaces for the cottages, which exceeds the parking requirements. Therefore, the project has sufficient onsite parking capacity and will not require use of offsite parking.*

- e) Hazards or barriers for pedestrians or bicyclists? (Source: 7)

*Discussion: The project does not have hazards or barriers for pedestrians or bicyclists.*

- f) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)?

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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(Sources: 1 & 8)

*Discussion: The project would not conflict with or otherwise affect adopted policies supporting alternative transportation.*

- g) Rail, waterborne or air traffic impacts?

*Discussion: The project could not affect rail, waterborne or air traffic.*

**BIOLOGICAL RESOURCES.** Would the proposal result in impacts to:

- Endangered, threatened or rare species or their habitats (including but not limited to: plants, fish, insects, animals, and birds)?

*Discussion: There are no endangered, threatened or rare species or their habitats located on the project site. Although the site is vacant, it has been disturbed over time by being grubbed, used for stockpiling, etc. It is an urban infill site, surrounded by development (and Hwy. 101 to the east) on all side. Thus, there could not be potential impacts to endangered, threatened or rare species or their habitats.*

- b) Locally designated species (e.g., heritage trees)?

*Discussion: There are several oak trees on the project sites. Per the Arborist Report, all trees will be preserved and protected on the project sites, and specific mitigation measures are recommended for potential encroachment into oak tree driplines and critical root zones, and for landscaping. With tree protection measures and recommendations incorporated as mitigation measures, the project will result in less than significant impacts on the existing oak trees.*

- c) Locally designated natural communities (e.g., oak forest, coastal habitat, etc.)?

*Discussion: See item b. above.*

- d) Wetland habitat (e.g., marsh, riparian and vernal pool)?

*Discussion: There are no wetland habitats on or near the project site.*

- e) Wildlife dispersal or migration corridors?

*Discussion: The site is not part of a wildlife dispersal or migration corridor.*

**VIII. ENERGY AND MINERAL RESOURCES.** Would the proposal:

- a) Conflict with adopted energy conservation plans? (Sources: 1 & 7)



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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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*Discussion: The structures will be designed and constructed according to applicable UBC codes and Title 24 energy conservation requirements, thus it will not conflict with adopted energy conservation plans.*

- b) Use non-renewable resources in a wasteful and inefficient manner? (Sources: 1 & 7)

*Discussion: The project will not use non-renewable resource in a wasteful and inefficient manner.*

- c) Result in the loss of availability of a known mineral resource that would be of future value to the region and the residents of the State? (Sources: 1 & 7)

*Discussion: The project is not located in an area of a known mineral resources that would be of future value to the region and the residents of the State.*

**IX. HAZARDS.** Would the proposal involve:

- a) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)?

*Discussion: The project will not result in a risk of accidental explosion or release of hazardous substances since the uses do not generally use these types of substances.*

- b) Possible interference with an emergency response plan or emergency evacuation plan? (Sources: 1 & 7)

*Discussion: The project will not interfere with an emergency response plan or emergency evacuation plan since it is not a designated emergency response location to be used for staging or other uses in an emergency.*

- c) The creation of any health hazard or potential hazards?

*Discussion: The project and future uses will not likely result in creating any health or other hazards.*

- d) Increased fire hazard in areas with flammable brush, grass, or trees?

*Discussion: The project site is not located in an area with the potential for increased fire hazards. The site will be required to be in compliance with City and County brush and grass clearance requirements.*

**X. NOISE.** Would the proposal result in:

- a) Increases in existing noise levels? (Sources: 1, 7, & 8)

*Discussion: The project will not likely result in a significant increase in operational noise levels. It may result in short-*  
*Initial Study-Page 13*

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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*term construction noise. However, construction noise will be limited to specific daytime hours per city regulations.*

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Exposure of people to severe noise levels? (Source: 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

*The project site is not located in the vicinity where it would expose people to severe noise levels.*

**XI. PUBLIC SERVICES.** Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Fire protection? (Sources: 1, 3, 6, & 7)                                   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Police Protection? (Sources: 1, 3, & 7)                                    | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Schools? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Maintenance of public facilities, including roads?<br>(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Other governmental services? (Sources: 1,3, & 7)                           | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

*Discussion: a.-e. The project applicant will be required to pay development impact fees as established by the city per AB 1600 to mitigate impacts to public services.*

**XII. UTILITIES AND SERVICE SYSTEMS.** Would the proposal result in a need for new systems or supplies, or substantial alterations to the following utilities:

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Power or natural gas? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Communication systems? (Sources: 1, 3, & 7)   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Local or regional water treatment or distribution facilities?<br>(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Sewer or septic tanks? (Sources: 1, 3, 7, & 8)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Storm water drainage? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f) Solid waste disposal? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| g) Local or regional water supplies? (Sources: 1, 3, & 7)                                | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

*Discussion: a.-g. The project will not result in the need for new systems or supplies, or result in substantial alterations to utilities and service systems.*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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**XIII. AESTHETICS.** Would the proposal:

- a) Affect a scenic vista or scenic highway? (Sources: 1, 3, & 7)

*Discussion: The project is not located in a scenic vista or scenic highway area.*

- b) Have a demonstrable negative aesthetic effect? (Sources: 1, 3, & 7)

*Discussion: The project is proposed to be designed with high quality materials and architectural design that is suitable to the site and will complement the area, and will not have a demonstrable negative aesthetic effect.*

- c) Create light or glare? (Sources: 1, 3, 7, & 8)

*Discussion: All light fixtures will be shielded and downcast as required per city regulations.*

**XIV. CULTURAL RESOURCES.** Would the proposal:

- a) Disturb paleontological resources? (Sources: 1, 3, & 7)

- b) Disturb archaeological resources? (Sources: 1, 3, & 7)

*Discussion: a.-b. The project site is not located in an area with know paleontological or archaeological resources. If these types of resources are found during grading and excavation, appropriate procedures will be followed including halting activities and contacting the County Coroner, and follow standard mitigation procedures.*

- c) Affect historical resources? (Sources: 1, 3, & 7)

*Discussion: There are no existing historical resources on the project site.*

- d) Have the potential to cause a physical change which would affect unique ethnic cultural values? (Sources: 1, 3, & 7)

*Discussion: The project is not proposed in a location where it could affect unique ethnic cultural values.*

- e) Restrict existing religious or sacred uses within the potential impact area? (Sources: 1, 3, & 7)

*Discussion: Discussion: There are no known religious or sacred uses on or near the project site.*

**XV. RECREATION.** Would the proposal:

- a) Increase the demand for neighborhood or regional parks or other recreational facilities? (Sources: 1, 3, & 7)

*Discussion: The project will not significantly affect the demand for parks and recreational facilities. The project*

**10 Environmental Checklist Form**

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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*complies with the build-out scenario of the General Plan which has adopted park and recreation facility thresholds per population. The applicant will need to pay associated park in-lieu fees for the residential units.*

b) Affect existing recreational opportunities? (Sources 1, 3, & 7)                       

*Discussion: The project will not affect existing recreational opportunities.*

**MANDATORY FINDINGS OF SIGNIFICANCE.**

Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? (Sources: 1 & 3)                       

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*Discussion: The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory.*

Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals? (Sources: 1 & 3)                       

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*Discussion: The project will not likely have a potential to achieve short-term, to the disadvantage of long-term environmental goals.*

Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.) (Sources: 1 & 3)                       

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*Discussion: The project will not result in significant cumulative impacts.*

Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly? (Sources: 1 & 3)                       

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*Discussion: The project will not result in substantial adverse environmental impacts on human beings, either directly or indirectly.*

## 11. EARLIER ANALYSIS AND BACKGROUND MATERIALS

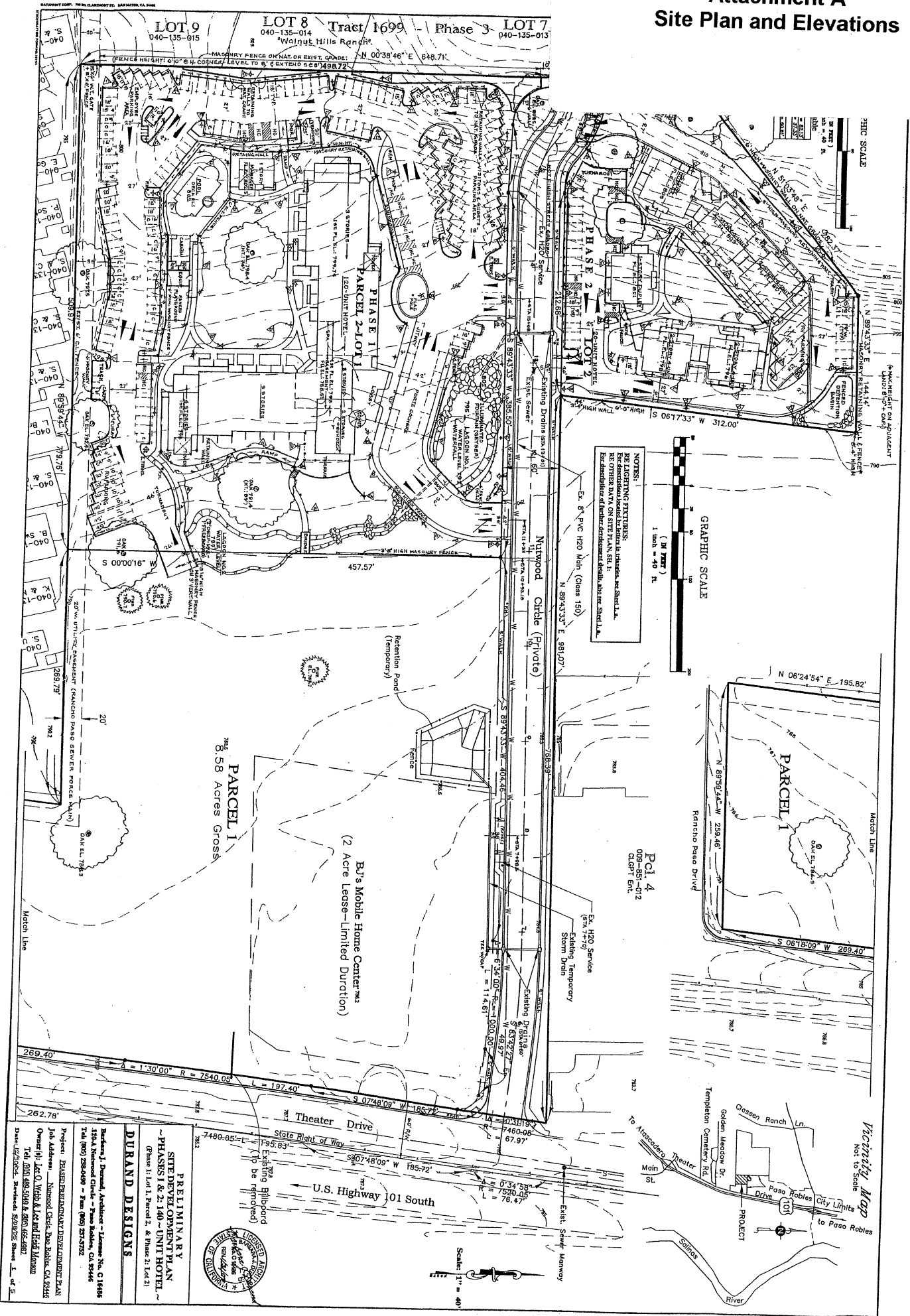
Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D). The earlier documents that have been used in this Initial Study are listed below.

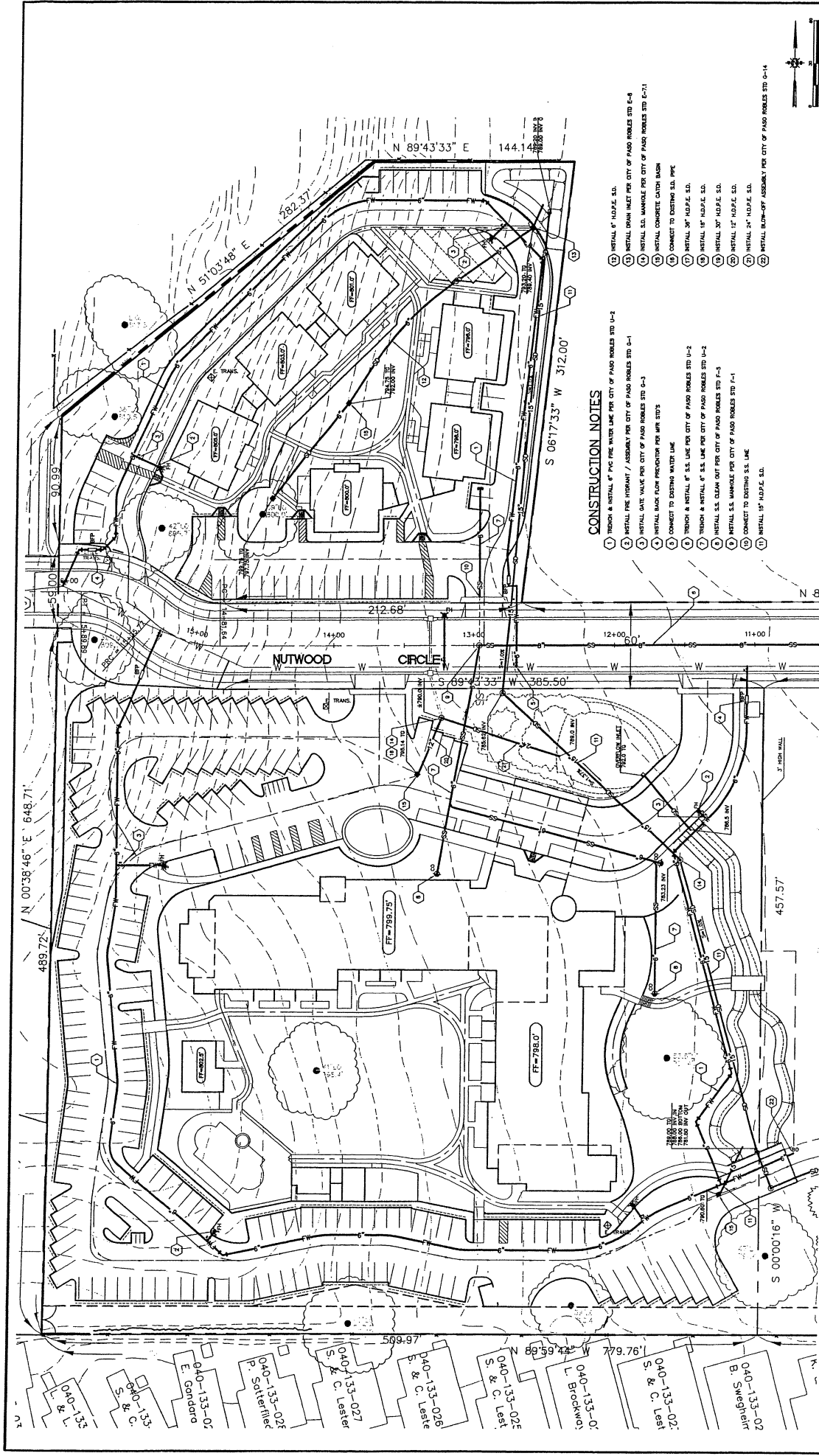
<b>Reference Number</b>	<b>Document Title</b>	<b>Available for Review At</b>
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
2	Seismic Safety Element for City of Paso Robles	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
3	Final Environmental Impact Report City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
4	Soil Survey of San Luis Obispo County, California Paso Robles Area	USDA-NRCS, 65 Main Street-Suite 108 Templeton, CA 93465
5	Uniform Building Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
6	City of Paso Robles Standard Conditions of Approval For New Development	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
7	City of Paso Robles Zoning Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
8	City of Paso Robles, Water Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
9	City of Paso Robles, Sewer Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
10	Federal Emergency Management Agency Flood Insurance Rate Map	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446

### **Attachments:**

Attachment A – Site Plan and Elevations  
Attachment B – Traffic Impact Study  
Attachment C – Arborist Report

# Attachment A Site Plan and Elevations





**CONSTRUCTION NOTES**

- 1 TROUGH & INSTALL 6" PVC FIRE WATER LINE PER CITY OF PASO ROBLES STD U-2
- 2 INSTALL FIRE HYDRANT / ASSEMBLY PER CITY OF PASO ROBLES STD U-1
- 3 INSTALL GATE VALVE PER CITY OF PASO ROBLES STD U-3
- 4 INSTALL BACK FLOW PREVENTER PER ARI STD'S
- 5 CONNECT TO EXISTING WATER LINE
- 6 TROUGH & INSTALL 6" U.S. LINE PER CITY OF PASO ROBLES STD U-2
- 7 TROUGH & INSTALL 6" U.S. LINE PER CITY OF PASO ROBLES STD U-2
- 8 INSTALL U.S. CLEAN OUT PER CITY OF PASO ROBLES STD F-3
- 9 INSTALL U.S. MANHOLE PER CITY OF PASO ROBLES STD F-1
- 10 CONNECT TO EXISTING U.S. LINE
- 11 INSTALL 15" HDPE S.D.
- 12 INSTALL 6" HDPE S.D.
- 13 INSTALL DRAIN INLET PER CITY OF PASO ROBLES STD E-4
- 14 INSTALL S.D. MANHOLE PER CITY OF PASO ROBLES STD E-7-1
- 15 INSTALL CONCRETE CATCH BASIN
- 16 CONNECT TO EXISTING S.D. PIPE
- 17 INSTALL 30" HDPE S.D.
- 18 INSTALL 18" HDPE S.D.
- 19 INSTALL 30" HDPE S.D.
- 20 INSTALL 12" HDPE S.D.
- 21 INSTALL 24" HDPE S.D.
- 22 INITIAL RUN-UP ASSEMBLY PER CITY OF PASO ROBLES STD G-14

FOR S.D. SEE SHEET C5.0

FOR S.S. SEE SHEET C5.0

**REVISIONS**

NO.	DATE	DESCRIPTION

DATE: \_\_\_\_\_

CITY ENGINEER: \_\_\_\_\_

**PROFESSIONAL ENGINEER**

**RUSSELL S. THOMPSON**

NO. 34348

CIVIL

STATE OF CALIFORNIA

EXPIRES 07/01/2008

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**WEBB & MONSON HOTEL BY DURAND DESIGNS**

**PRELIMINARY UTILITY PLAN**

**CITY OF PASO DE ROBLES, CA.**

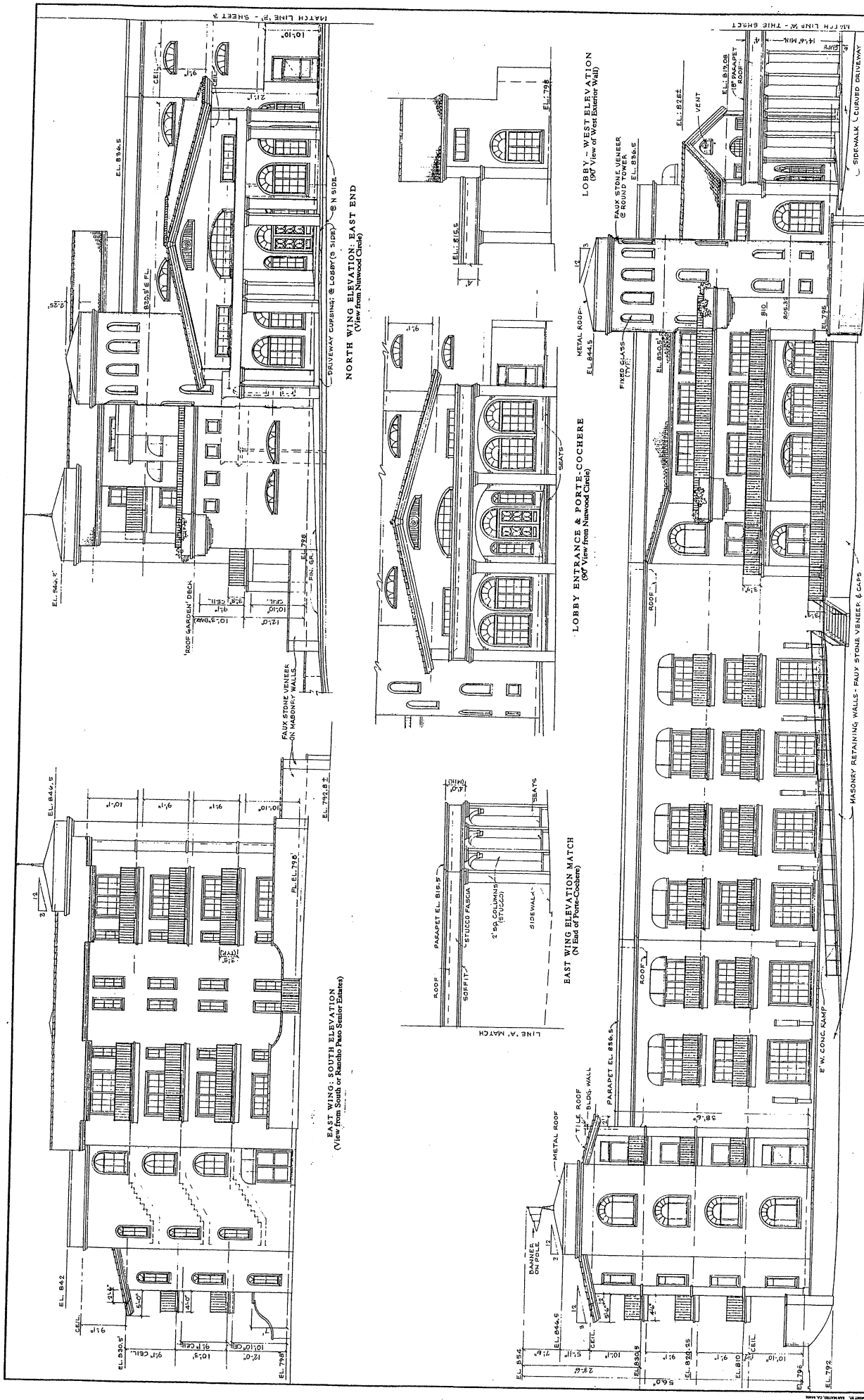
PROJECT NO: 05-014  
SHEET NO: 4 OF 5

DATE: 7-5-05

SCALE: C40

DESIGNED BY: [Signature]  
CHECKED BY: [Signature]  
DATE: 05-014  
SHEET NO: 4 OF 5

REGISTERED PROFESSIONAL ENGINEER  
R. THOMPSON CONSULTING, INC.  
14000 CALIFORNIA AVENUE  
SAN ANTONIO, CA 78240  
(214) 480-0000



**DURAND DESIGNS**  
 Preliminary Development Plan  
 ~ HOTEL EXTERIORS: 120-UNITS ~  
 (Phase I: Lot 1, Parcel 2 of Parcel Map PR-04-0471)  
 ~ Scale: 1/8" = 1' ~

Project: RAISED PREAMBULAR DEVELOPMENT PLAN  
 Job Address: Nimwood Circle, Paso Robles, CA 93446  
 Owner(s): Lot 1, Parcel 2 of Parcel Map PR-04-0471  
 Tel: 805.468.5000 & 805.468.8887  
 Desc: 4/25/05, Revised: 8/22/05 Sheet 2 of 5

EAST WING ELEVATION  
 (View from East Garden & Theatre Drive)

EAST WING: SOUTH ELEVATION  
 (View from South or Rancho Paso Senior Estates)

NORTH WING ELEVATION: EAST END  
 (View from Nimwood Circle)

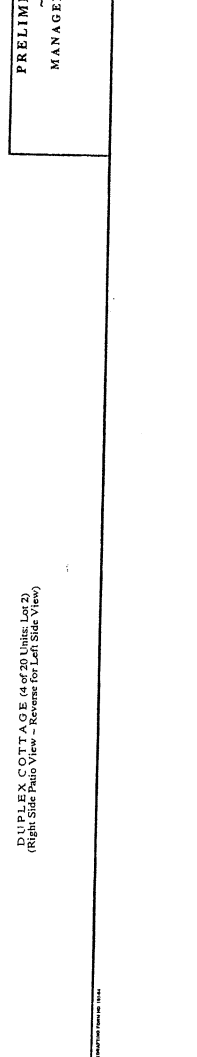
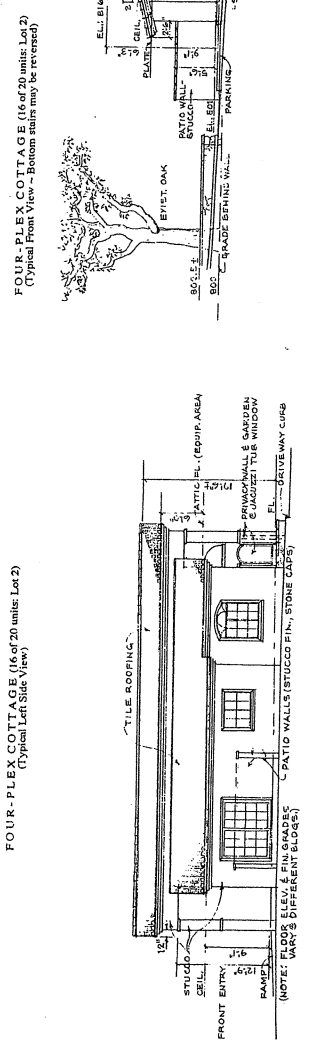
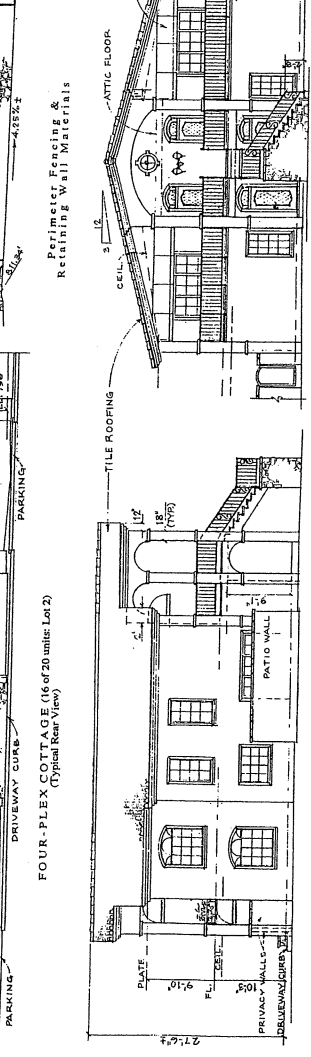
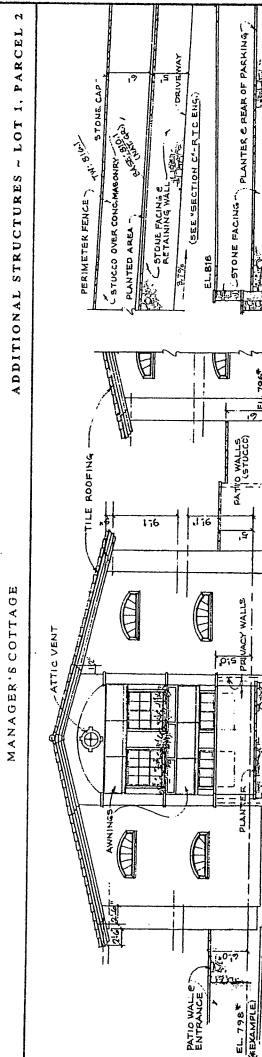
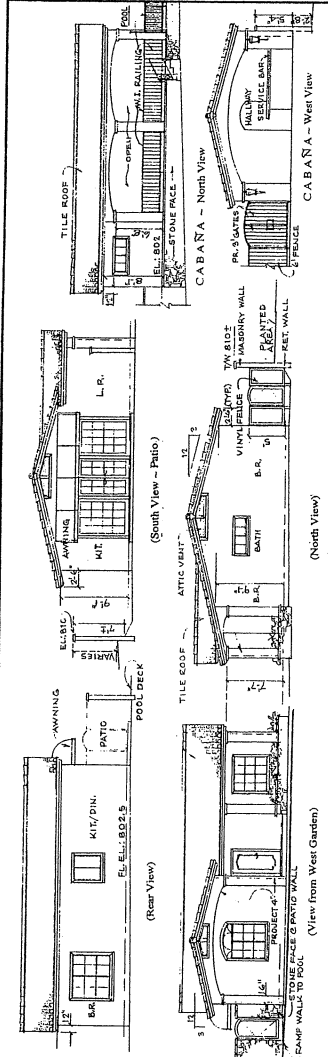
EAST WING ELEVATION MATCH  
 (N End of Porte-Cochere)

LOBBY ENTRANCE & PORTE-COCHERE  
 (90° View from Nimwood Circle)

LOBBY - WEST ELEVATION  
 (90° View of West Exterior Wall)







**PRELIMINARY DEVELOPMENT PLAN**  
 ~ HOTEL EXTERIORS ~  
 MANAGERS UNIT + 20 COTTAGE SUITES  
 (Phases 1 and 2 of PD 04-024)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**  
 Rick Jones, J. Durand, Architect - License No. C 16485  
 1204 Nutwood Circle - Palo Alto, CA 94406  
 Tel: (650) 253-5053 & (650) 852-4877  
 Fax: (650) 253-4896 - Fax: (650) 257-3751

Project: PHASE 2 PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Nutwood Circle, Palo Alto, CA 94406  
 Owner(s): J.C.O. Webb & Lee and Heidi Monson  
 Date: 6/27/05 - Revised: 8/25/05 Sheet 4 of 5



**ASSOCIATED TRANSPORTATION ENGINEERS**

100 N. Hope Avenue, Suite 4, Santa Barbara, CA 93101

Richard L. Pool, P.E.  
Scott A. Schell, AICP

**Paso Robles**

**NOV 14 2006**

**Planning Division**

06113R01.WPD

November 13, 2006

John Falkenstien  
City of Paso Robles  
1000 Spring Street  
Paso Robles, CA 93446

***TRAFFIC IMPACT STUDY FOR THE DURAND PROJECT, CITY OF PASO ROBLES***

Associated Transportation Engineers (ATE) has prepared the following traffic study for the Durand Project, proposed in the southern portion of the City of Paso Robles. The project is proposing to construct a 120-room hotel on the southwest corner of the Theater Drive/Nutwood Circle intersection plus 20 hotel guest suites across the street on Nutwood Circle.

**EXISTING CONDITIONS**

Table 1 shows the Existing Weekday Midday, Weekday P.M., Friday P.M., and Saturday P.M. peak hour levels of service for the intersections comprising the Route 46 (W)/Route 101 interchange. Peak hour traffic volumes for the interchange were collected in April and June 2005. The counts include the number of trucks using the interchange.

**Table 1**  
**Route 46 (W)/Route 101 Interchange**  
**Existing Peak Hour Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	34.1/LOS C	35.6/LOS D	38.8/LOS D	36.4/LOS D
Rt 46(W)/Rt 101 NB	14.5/LOS B	14.6/LOS B	19.7/LOS B	17.7/LOS B
Rt 46(W)/Ramada	15.8/LOS C	15.5/LOS C	18.6/LOS B	14.2/LOS B

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection operates at LOS C during the Weekday Midday period and LOS D during the Weekday P.M., Friday P.M., and Saturday P.M. peak hour periods. The two intersections comprising the east side of the interchange operate at LOS B-C.

### NEAR-TERM CONDITIONS

Near-term traffic conditions were forecasted for the interchange assuming the additional traffic generated by the approved developments in the vicinity of the interchange, along with the near-term improvements planned for the interchange. The following section details the near-term level of service forecasting for the interchange.

### Approved Projects

There are several approved projects that will add traffic to the study-area street system in the near-term. Table 2 shows the approved projects in the area.

**Table 2  
Route 46 (W)/Route 101 Interchange - Approved Projects**

Project	Land Use	Size
Target Center <sup>a</sup>	Shopping Center	17,000 SF
La Vorgna Storage	Storage	2.8 Acres
McDonalds/Chevron	Fast-Food Restaurant Gas Station w/ Car Wash	3,152 SF 12 Pumps/1 Wash
Gheza Mini-Storage	Storage	3.75 Acres
Theater Drive Retail	Mixed Retail Self-Storage	39,650 SF 62,000 SF
Bellesara Suites Project	Lodging	62-Unit Hotel
1331 Vendels Circle - Benny Simmons	Light Industry	6,000 SF
1160 Ramada Drive - True Tube	Light Industry	6,900 SF
1375 Ramada Drive - Lavorgna	Light Industry	46,684 SF
1500 Ramada Drive - Pokrajac	Light Industrial/Warehouse	42,800 SF
The Inns at Vintner's Village Project	Lodging	138 rooms

<sup>a</sup> Remaining square footage under original shopping center approval.

**Route 46 (W)/Route 101 Improvements**

The City commissioned a traffic study of the interchange in the Summer of 2002. That study analyzed traffic conditions at the interchange and included improvements to provide additional capacity at the interchange in the near term. The near-term improvements that are being constructed and are expected to be completed in late 2006-early 2007 include:

- 1) Modify Route 101 SB Off-Ramp. Lengthen the ramp storage lanes to provide 500-550 feet of storage. The off-ramp contains one left-turn lane and two right-turn lanes. The inside right-turn lane is designated for southbound Theater Drive and the outside lane is designated for Route 46 (W) & Vine Street.
- 2) Modify Curb Return on Northeast Corner of SB Off-Ramp. Modify the curb return to accommodate truck turns.

- 3) Signalize Route 46 (W)/Ramada. Install traffic signals at this location.
- 4) Signal Interconnect & Coordination. Provide for interconnection of all of the traffic signals in the interchange and provide a timing plan for signal coordination.

### Levels of Service

Table 3 shows the near-term level of service forecasts for the Route 46 (W)/Route 101 interchange assuming the near-term traffic volume projections for the Existing + Approved Projects scenario and the near-term improvements listed above.

**Table 3**  
**Route 46 (W)/Route 101 Interchange - Near-Term Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	35.8/LOS D	40.9/LOS D	57.7/LOS E	45.5/LOS D
Rt 46(W)/Rt 101 NB/Ramada	18.4/LOS B	19.9/LOS B	21.7/LOS C	21.6/LOS C

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB, and Rt 46(W)/Rt 101 NB and Rt 46(w)/Ramada are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection is forecast to operate at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period with the near-term developments and improvements. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

### PROJECT IMPACTS

#### Trip Generation

Trip generation estimates were calculated for the Durand Project based on ITE, Caltrans and SANDAG rates. Table 4 summarizes the trip generation calculations for the Weekday Midday, Weekday P.M., Friday P.M., and Saturday P.M. peak hour periods.

**Table 4**  
**Project Trip Generation**

Use	Size	Peak Hour Period	Trips
Hotel	120 Rooms 20 Suites	Weekday Midday	134
		Weekday P.M.	83
		Friday P.M.	83
		Saturday P.M.	101

Trip generation rates derived from ITE, SANDAG & Caltrans studies.

The trips that will be generated by the project were distributed and assigned to the Route 46 (W)/Route 101 Interchange using the distribution percentages shown in Table 5. These percentages were developed from marketing data/traffic studies of other commercial projects in the area, as well as consideration of the street system and the surrounding residential and commercial centers. Most of the traffic is expected to/from Route 101 (65%) with the remainder via the other surface streets in the area. 12% of the traffic will be local and/or link to other uses in the Theater Drive area.

**Table 5**  
**Project Trip Distribution**

Origin/Destination	Direction	Percent
Route 101	North	35%
Route 101 <sup>a</sup>	South	30%
Vine Street	North	10%
SR 46 West	West	5%
Ramada Drive n/o SR 46 West	East	2%
Ramada Drive s/o SR 46 West	East	1%
Main Street <sup>b</sup>	South	5%
Local	—	12%
<b>Total</b>		<b>100%</b>

<sup>a</sup> Traffic to/from south on Route 101 assigned to the Main Street interchange.

<sup>b</sup> Traffic to/from south on Main Street assigned to the Main Street interchange.

**NEAR-TERM + PROJECT LEVELS OF SERVICE**

Table 6 shows the near-term + project level of service forecasts for the Route 46 (W)/Route 101 Interchange. The levels of service assume the traffic volume projections for the Existing + Approved + Project condition.

**Table 6**  
**Route 46 (W)/Route 101 Interchange**  
**Near-Term + Project Peak Hour Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	39.3/LOS D	41.9/LOS D	61.1/LOS E	46.4/LOS D
Rt 46(W)/Rt 101 NB/Ramada	17.8/LOS B	18.2/LOS B	21.8/LOS C	22.6/LOS C

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB, and Rt 46(W)/Rt 101 NB and Rt 46(w)/Ramada are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection is forecast to operate at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

**ROUTE 46 (W)/ROUTE 101 SB OFF-RAMP QUEUING**

The maximum queues forecasted on the Route 101 SB off-ramp will occur during the Friday and Saturday P.M. peak hour periods. With the additional traffic from the approved projects and the Durand Project, the maximum queues are forecasted at 405 feet during the Friday P.M. peak hour period and 435 feet during the Saturday P.M. peak hour period assuming the near-term improvements that are being constructed. The near-term improvements include lengthening the ramp to provide 550 feet of storage. Thus, the traffic on the southbound off-ramp could be accommodated with the additional storage provided by the near-term improvements.



## PROJECT SHARE OF NEAR-TERM IMPROVEMENTS

Table 7 shows the percentage contribution of traffic at the Route 46 (W)/Route 101 interchange for each of the approved projects that would contribute to the improvements, including the Durand Project.

**Table 7**  
**Route 46 (W)/Route 101 Interchange**  
**Traffic Contributions from Near-Term Projects**

Project	Traffic Contribution	% Share
McDonalds/Chevron	214 PHT	39.3%
Gheza Mini-Storage	15 PHT	2.8%
Theater Drive Retail/Storage	71 PHT	13.0%
Bellesara Suites	34 PHT	6.2%
1331 Vendels Circle Project - Benny Simmons	6 PHT	1.1%
1160 Ramada Drive Project - True Tube	7 PHT	1.3%
1375 Ramada Drive - Lavorgna	46 PHT	8.4%
1500 Ramada Drive - Pokrajac	30 PHT	5.5%
Inns at Vintner's Village Project	77 PHT	14.1%
Durand Project	45 PHT	8.3%
<b>Total</b>	<b>545 PHT</b>	<b>100%</b>

Traffic Contribution based on weekday peak hour trips (PHT) entering the interchange.

The traffic generated by the Durand Project would be 8.3% of the traffic that will be added by the approved projects.

**SUMMARY**


The projected traffic from the existing plus approved projects will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday

P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

The addition of the projected Durand Project traffic will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

The Durand Project's portion of the traffic that will be added to the interchange by the approved project is 8.3%.

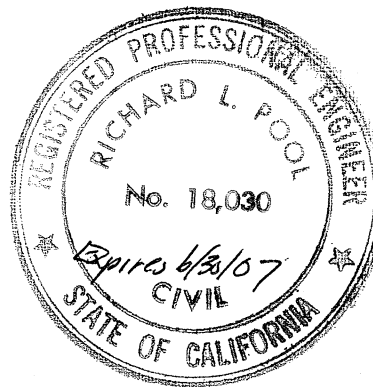
Associated Transportation Engineers



By: Richard L. Pool, P.E.  
President

RLP/DLD/LDH

attachments



**A & T ARBOR**


P.O. BOX 1311 TEMPLETON, CA 93465

**Tree Preservation Plan  
Addendum  
Durand Project  
Paso Robles, CA**

**8-01-05**

**Prepared by A & T Arborists  
and Vegetation Management**

**Chip Tamagni  
Certified Arborist #WE 6436-A**



**Steven Alvarez  
Certified Arborist #WE 511-A**

**RECEIVED**  
AUG 05 2005  
Community Development

Tract # \_\_\_\_\_

PD # 04-024

Building Permit # \_\_\_\_\_

**Project Description:** This project involves the construction of a hotel, cottages and accompanying parking areas located off theater drive in Paso Robles, CA. The area currently has nine valley oak trees (*Quercus lobata*) and none are planned for removal. There are two additional valley oaks located on the property adjacent to lot #2.

**Specific Mitigations Pertaining to the Project:** As outlined on the spreadsheet, all trees must have protective fencing in place and inspected by the arborists prior to grading. All impacts (grading for curbing, parking, retaining walls and footings) shall be monitored and any necessary root pruning, wetting, root fungicide application will be completed by the project arborists. The arborists shall be called a minimum of 48 hours before any needed monitoring. The arborists shall also be called out to monitor the retaining wall trenching near the neighboring trees near lot #2. The crz encroachment for tree #9 should not impact the tree long-term. The grade for this tree shall not be changed within the fencing nor shall any grading contribute to a ponding condition around the trunk.

The term “critical root zone” or CRZ is an imaginary circle around each tree. The radius of this circle (in feet) is equal to the diameter (in inches) of the tree. For example, a 10 inch diameter tree has a critical root zone with a ten foot radius from the tree. Working within the CRZ usually requires mitigations and/or monitoring by a certified arborist.

All trees potentially impacted by this project are numbered and identified on both the grading plan and the spreadsheet. Trees are numbered on the grading plans and in the field with an aluminum tag. Tree protection fencing is shown on the grading plan. In the field oak trees to be saved have yellow tape attached to the tag (no removals for this project). Both critical root zones and drip lines are outlined on the plans.

If pruning is necessary for building, road or driveway clearance, removal of limbs larger than 6 inches in diameter will require a city approved permit along with a deposit paid in advance (to the City of Paso Robles). The city will send out a representative to approve or deny the permit. Only 25% of the live crown may be removed. At this time, no trees appear to need any building clearance pruning.

### **Tree Rating System**

A rating system of 1-10 was used for visually establishing the general health and condition of each tree on the **spreadsheet**. The rating system is defined as follows:

<b><u>Rating</u></b>	<b><u>Condition</u></b>
0	Deceased
1	Evidence of massive past failures, extreme disease and is in severe decline.
2	May be saved with attention to class 4 pruning, insect/pest eradication and future monitoring.

- 3 Some past failures, some pests or structural defects that may be mitigated by class IV pruning.
- 4 May have had minor past failures, excessive deadwood or minor structural defects that can be mitigated with pruning.
- 5 Relatively healthy tree with little visual, structural and/or pest defects and problems.
- 6 Healthy tree that probably can be left in its natural state.
- 7-9 Has had proper arboricultural pruning and attention or have no apparent structural defects.
- 10 Specimen tree with perfect shape, structure and foliage in a protected setting (i.e. park, arboretum).

Aesthetic quality is defined as follows:

- **poor** - tree has little visual quality either due to severe suppression from other trees, past pruning practices, location or sparse foliage
- **fair** - visual quality has been jeopardized by utility pruning/obstructions or partial suppression and overall symmetry is average
- **good** - tree has good structure and symmetry either naturally or from prior pruning events and is located in an area that benefits from the trees position
- **excellent** - tree has great structure, symmetry and foliage and is located in a premier location. Tree is not over mature.

All trees on the site are relatively the same size and age (mature). Aesthetically speaking, the project has been designed to positively accent the trees. They all rate from good to excellent. The trees on site have a useful life expectancy of 50 to 80 years.

The following mitigation measures/methods must be fully understood and followed by anyone working within the critical root zone of any native tree. Any necessary clarification will be provided by us (the arborists) upon request.

**1.** It is the responsibility of the **owner or project manager** to provide a copy of this tree protection plan to any and all contractors and subcontractors that work within the critical root zone of any native tree and confirm they are trained in maintaining fencing, protecting root zones and conforming to all tree protection goals. It is highly recommended that each contractor sign and acknowledge this tree protection plan.

**2.** Any future changes (within the critical root zone) in the project will need Project Arborist review and implementation of potential mitigation measures before any said changes can proceed.

**3. Fencing:** The proposed fencing shall be shown in orange ink on the grading plan. It must be a minimum of 4' high chain link, snow or safety fence staked (with t posts 8 feet on center) at the edge of the critical root zone or line of encroachment for each tree or group of trees. The fence shall be up before any construction or earth moving begins. The owner shall be responsible for maintaining an erect fence throughout the construction period. The arborist(s), upon notification, will inspect the fence placement once it is erected. After this time, fencing shall not be moved without arborist inspection/approval. If the orange plastic fencing is used, a minimum of four zip ties

shall be used on each stake to secure the fence. All efforts shall be made to maximize the distance from each saved tree. Weather proof signs shall be permanently posted on the fences every 50 feet, with the following information:

<p><b>Tree Protection Zone</b> No personnel, equipment, materials, and vehicles are allowed Do not remove or re-position this fence without calling: A &amp; T Arborists 434-0131</p>
---

4. **Soil Aeration Methods:** Soils within the critical root zone that have been compacted by heavy equipment and/or construction activities must be returned to their original state before all work is completed. Methods include water jetting, adding organic matter, and boring small holes with an auger (18" deep, 2-3' apart with a 2-4" auger) and the application of moderate amounts of nitrogen fertilizer. The arborist(s) shall advise.
5. **Chip Mulch:** All areas within the critical root zone of the trees that can be fenced shall receive a 4-6" layer of chip mulch to retain moisture, soil structure and reduce the effects of soil compaction.
6. **Trenching Within Critical Root Zone:** All trenching within the critical root zone of native trees shall be **hand dug**. All major roots shall be avoided whenever possible. All exposed roots larger than 1" in diameter shall be clean cut with sharp pruning tools and not left ragged. A **Mandatory** meeting between the arborists and grading contractor(s) must take place prior to work start.
7. **Grading Within The Critical Root Zone:** Grading should not encroach within the critical root zone unless authorized. Grading should not disrupt the normal drainage pattern around the trees. Fills should not create a ponding condition and excavations should not leave the tree on a rapidly draining mound.
8. **Exposed Roots:** Any exposed roots shall be re-covered the same day they were exposed. If they cannot, they must be covered with burlap or another suitable material and wetted down 2x per day until re-buried.
9. **Paving Within The Critical Root Zone:** Pervious surfacing is preferred within the critical root zone of any native tree. If pavers are required, the areas are outlined on the grading plans. Pavers must be interlocking with a minimum of 10% void space backfilled with pea gravel. Geo textile fabric shall be permeable. Depending on use within the CRZ, pavers may or may not be required. Parking spots do not require pavers as they provide a leaching area for oil and antifreeze. The non-parking area within the crz of tree #5 will require pavers as described above.
10. **Equipment Operation:** Vehicles and all heavy equipment shall not be driven under the trees, as this will contribute to soil compaction. Also there is to be no parking of equipment or personal vehicles in these areas. All areas behind fencing are off limits unless pre-approved by the arborist.

**11. Existing Surfaces:** The existing ground surface within the critical root zone of all oak trees shall not be cut, filled, compacted or pared, unless shown on the grading plans **and** approved by the arborist.

**12. Construction Materials And Waste:** No liquid or solid construction waste shall be dumped on the ground within the critical root zone of any native tree. The critical root zone areas are not for storage of materials either.

**13. Arborist Monitoring:** An arborist shall be present for selected activities (trees identified on spreadsheet and items bulleted below). The monitoring does not necessarily have to be continuous but observational at times during these activities. It is the responsibility of the **owner(s) or their designee** to inform us prior to these events so we can make arrangements to be present. All monitoring will be documented on the field report form which will be forwarded to the project manager and the City of Paso Robles Planning Department. Monitoring is \$75.00/hour, 2 hour minimum. A contract shall be signed prior to work start.

- pre-construction fence placement inspection
- all grading and trenching identified on the spreadsheet
- any other encroachment the arborist feels necessary

**14. Pre-Construction Meeting:** An on-site pre-construction meeting with the Arborist(s), Owner(s), Planning Staff, and the earth moving team shall be required for this project. Prior to final occupancy, a letter from the arborist(s) shall be required verifying the health/condition of all impacted trees and providing any recommendations for any additional mitigation. The letter shall verify that the arborist(s) were on site for all grading and/or trenching activity that encroached into the critical root zone of the selected native trees, and that all work done in these areas was completed to the standards set forth above.

**15. Pruning** Class 4 pruning includes-Crown reduction pruning shall consist of reduction of tops, sides or individual limbs. A trained arborist shall perform all pruning. No pruning shall take more than 25% of the live crown of any native tree. Any trees that may need pruning for road/home clearance shall be pruned **prior** to any grading activities to avoid any branch tearing.

**16. Landscape:** All landscape within the critical root zone shall consist of drought tolerant or native varieties. Lawns shall be avoided. All irrigation trenching shall be routed around critical root zones, otherwise above ground drip-irrigation shall be used. It is the owner's responsibility to notify the landscape contractor regarding this mitigation.

**17. Utility Placement:** All utilities, sewer and storm drains shall be placed down the roads and driveways and when possible outside of the critical root zones. The arborist shall supervise trenching within the critical root zone. **All trenches in these areas shall be exposed by air spade or hand dug with utilities routed under/over roots larger than 3 inches in diameter.**

**18. Fertilization and Cultural Practices:** As the project moves toward completion, the arborist(s) may suggest either fertilization and/or mycorrhiza applications that will benefit tree health. Mycorrhiza offers several benefits to the host plant, including faster growth, improved nutrition, greater drought resistance, and protection from pathogens.

The included spreadsheet includes trees listed by number, species and multiple stems if applicable, scientific name, diameter and breast height (4.5'), condition (scale from poor to excellent), status (avoided, impacted, removed, exempt), percent of critical root zone impacted, mitigation required (fencing, root pruning, monitoring), construction impact (trenching, grading), recommended pruning and individual tree notes.

If all the above mitigation measures are followed, we feel there will be no long-term significant impacts to the native trees.

Please let us know if we can be of any future assistance to you for this project.

Steven G. Alvarez  
Certified Arborist #WC 0511

Chip Tamagni  
Certified Arborist #WE 6436-A



DURAND DESIGNS  
NUTWOOD CIRCLE  
HOTEL SITE

TREE PROTECTION SPREAD SHEET

1	2	3	4	5	6	7	8	9	10	11
TREE #	TREE SPECIES	TRUNK DBH	TREE CONDITION	CONST STATUS	D RIP-LINE % IMPACT	CONST IMPACT	MITIGATION PROPOSAL	MONT REQUIRED	PRUNING CLASS	FIELD NOTES
1	0	0	0	0	0%	0	0	0	0	0
2	0	0	0	0	0%	0	0	0	0	0
3	VO	47"	8	I	50%	G,C	F,M	Y	C-4	MOVE VEH PKING, NAT PLTS
4	VO	53.5"	6	I	40%	G,C	F,M	Y	C-4	LAGOON ADJUSTMENT?
5	VO	39"	4	I	40%	G,C	F,M	Y	C-4	RD, PK, TRASH, PAVERS
6	VO	40"	4	I	30%	G,C	F,M	Y	C-4	PK, DEADWD PRUNING A MUST*
7	VO	41"	6	I	100%	G,C	F,M	Y	NONE	NO IRR, NO LAWN, NATIVE ONLY
8	VO	39"	4	I	15%	G,C	F,M	Y	NONE	PARKING, CURB
9	VO	39"	5	I	80%	G,C	F,M	Y	NONE	PK, CONST, GRADING
10	VO	42"	4	I	80%	G,C	F,M	Y	NONE	PK, CONST, GRADING
11	VO	36	3	I	20%	G,C	F,M	Y	NONE	PK, GRADING, CONST

→ 50% is landscape impact (drought tolerant)

- 1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
- 2 = TREE TYPE: COMMON NAME (E.W.O.= WHITE OAK)
- 3 = TRUNK DIAMETER @ 4ft
- 4 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
- 5 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
- 6 = DRIP-LINE: PERCENT OF IMPACTED DRIP-LINE
- 7 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
- 8 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTING PRUNING, PRUNING
- 9 = ARBORIST MONITORING REQUIRED: YES/NO
- 10 = PERSCRIBED PRUNING: CLASS 1-4
- 11 = FIELD NOTES

\*TREE #6 HAS THREE LIMBS THAT ARE HAZARDIST AND OVER HANGING THE NEIGHBORING TRAILOR PARK THE LIMBS SHOULD BE REMOVED AS SOON AS POSSIBLE

**RESOLUTION NO.**

**A RESOLUTION OF  
THE PLANNING COMMISSION OF THE CITY OF EL PASO DE ROBLES  
PLANNED DEVELOPMENT 04-024 FOR THE DURRAND HOTEL AND SUITES  
2805 THEATRE DRIVE (ACCESSED FROM NUTWOOD CIRCLE)  
APPLICANTS - LEE MONSON AND LEE WEBB  
(APNs: 009-851-016 AND -023)**

**WHEREAS**, Planned Development 04-024 has been filed by Lee Monson and Lee Webb; and

**WHEREAS**, Planned Development 04-024 is a proposal to construct a 120 unit hotel and 20 guest cottage suites; and

**WHEREAS**, the General Plan land use designation is RC (Regional Commercial) and the Zoning District is Highway Commercial – Planned Development (C2-PD); and

**WHEREAS**, the project complies with the guidelines and standards of the Zoning Ordinance and is designed to be compatible with the surrounding development; and

**WHEREAS**, the Planning Commission held a duly noticed public hearing on February 27, 2007 for this commercial hotel project, to accept public testimony on the Planned Development application, PD 04-024 and associated environmental review; and

**WHEREAS**, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City's Procedures for Implementing CEQA, an Initial Study was prepared and circulated for public review and comment; and

**WHEREAS**, based on the information and analysis contained in the Initial Study, a determination has been made that the proposed commercial hotel project will not result in significant environmental impacts and it is appropriate for the Planning Commission to adopt a Negative Declaration, which is included in a separate resolution; and

**WHEREAS**, based upon the facts and analysis presented in the staff report and the attachments thereto, the public testimony received, and subject to the Conditions of Approval listed below, the Planning Commission makes the following findings:

1. The project is consistent with the adopted codes, policies, standards and plans of the City, and implements the goals of the City's General Plan and Economic Strategy; and
2. The proposed development plan will not be detrimental to the health, safety, morals, comfort, convenience and general welfare of the residents and or businesses in the surrounding neighborhood, or be injurious or detrimental to property and improvements in the neighborhood or to the general welfare of the City; and
3. The proposed development plan accommodates the aesthetic quality of the City as a whole, especially where development will be visible from the gateways to the City, scenic corridors; and the public right-of-way; and
4. The proposed development plan is compatible with, and is not detrimental to, surrounding land uses and improvements, provides an appropriate visual appearance, and contributes to the mitigation of any environmental and social impacts; and

5. The proposed development plan is compatible with existing scenic and environmental resources such as hillsides, oak trees, vistas, etc.; and
6. The proposed development plan contributes to the orderly development of the City as a whole.

**NOW, THEREFORE, BE IT RESOLVED**, that the Planning Commission of the City of El Paso de Robles does hereby approve Planned Development 04-024, subject to the following conditions:

**STANDARD CONDITIONS:**

1. This project shall comply with the checked standard Conditions of Approval, attached hereto as Exhibit “A” and incorporated herein by reference.

**SITE SPECIFIC CONDITIONS:**

NOTE: In the event of conflict or duplication between standard and site-specific conditions, the site-specific condition shall supersede the standard condition.

2. The project shall be constructed in substantial conformance with the Conditions of Approval established by this Resolution and it shall be constructed in substantial conformance with the following Exhibits:

<b>EXHIBIT</b>	<b>DESCRIPTION</b>
A	Standard Conditions of Approval
B	Site Plan
C	Elevations

3. This PD 04-024 allows for development and operation of a 120 room hotel, 20 guest cottage suites, and ancillary uses as permitted in the Zoning Ordinance on the project site.
5. The project shall be designed and constructed to be in substantial conformance with the site plan and elevations approved with this resolution. The high quality of craftsmanship and design of the architecture and materials, site plan layout, amenities and other project details shall not be modified in the future to accommodate corporate design features. Materials indicated on the project colors and materials board and lighting plan shall be utilized for this project.
6. This PD 04-024 is valid for a period of two (2) years from approval. Unless permits have been issued and site work has begun, the approval of PD 04-024 shall expire on February 27, 2009. The Planning Commission may extend this expiration date for an additional three (3) years if a time extension application has been filed with the City along with the fees before the expiration date.
7. Prior to issuance of certificates of use and occupancy, the property-owner or authorized agent is required to pay the City’s Development Impact Fees.
8. No underground or aboveground storage of hazardous materials shall be allowed on-site without first obtaining City approval.
9. No storage of trash cans or recycling bins shall be permitted within the public right-of-way.
10. Enhanced pedestrian connections between the main hotel project and the cottages shall be required as approved by the Development Review Committee, and may include a pedestrian crossing with colored, stamped concrete, bulb-outs, and unified landscaping and lighting facilities for both sides of Nutwood Circle.

11. All existing and new overhead utilities shall be placed underground, except as otherwise exempted by City codes.
12. All oak trees on the project site shall be preserved and protected utilizing the oak tree protection methods identified in the report prepared by the project arborist.
13. A solid masonry wall six feet in height shall be constructed along the south side of the property boundary for the main hotel site, and it shall be continued along the west boundary. The cottage site shall utilize the same perimeter wall design. The walls shall incorporate smooth stucco wall finish consistent with the buildings exterior finish materials and/or incorporate rock wall details, with wall cap and pilasters spaced a minimum of 10 feet apart.
14. Temporary construction noise levels in excess of 60 decibels shall be restricted to the daylight hours of 7am to 6pm. Noise levels shall be measured or monitored from site boundaries or the nearest adjoining residential use to determine compliance.
15. The project shall be in compliance the following recommendations of the San Luis Obispo County Air Pollution Control District so as to minimize creation of fugitive dust and other emission resulting from use of construction equipment as follows:

**CONSTRUCTION PHASE MITIGATION:**

**Dust Control Measures**

Construction activities can generate fugitive dust, which could be a nuisance to local residents and businesses in close proximity to the proposed construction site. Dust complaints could result in a violation of the District's 402 "Nuisance" Rule. Due to this project's proximity to neighboring commercial uses the APCD conditions this project to comply with all applicable air quality regulations pertaining to the control of fugitive dust (PM10) as contained in section 6.5 of the Air Quality Handbook. **All site grading and demolition plans noted shall list the following regulations:**

- a. Reduce the amount of the disturbed area where possible.
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (nonpotable) water should be used whenever possible.
- c. All dirt stock pile areas should be sprayed daily as needed.
- d. Permanent dust control measures identified in the approved project revegetation and landscape plans should be implemented as soon as possible following completion of any soil disturbing activities.
- e. Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading should be sown with a fast germinating native grass seed and watered until vegetation is established.
- f. All disturbed soil areas not subject to revegetation should be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD.
- g. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.
- h. Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.
- i. All trucks hauling dirt, sand, soil, or other loose materials are to be covered or should maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114.
- j. Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site.
- k. Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water should be used where feasible.

All PM10 mitigation measures required should be shown on grading and building plans. In addition, the contractor or builder should designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress. **The name and telephone number of such persons shall be provided to the APCD prior to land use clearance for map recordation and finished grading of the area.**

#### Naturally Occurring Asbestos

The project site is located in a candidate area for Naturally Occurring Asbestos (NOA), which has been identified as a toxic air contaminant by the California Air Resources Board (ARB). Under the ARB Air Toxics Control Measure (ATCM) for Construction, Grading, Quarrying, and Surface Mining Operations, **prior to any grading activities at the site, the project proponent shall ensure that a geologic evaluation is conducted to determine if NOA is present within the area that will be disturbed.** If NOA is not present, an exemption request must be filed with the District (see Attachment 1). If NOA is found at the site the applicant must comply with all requirements outlined in the Asbestos ATCM. This may include development of an Asbestos Dust Mitigation Plan and an Asbestos Health and Safety Program for approval by the APCD. Please refer to the APCD web page at <http://www.slocleanair.org/business/asbestos.asp> for more information or contact Karen Brooks of our Enforcement Division at 781-5912.

#### Demolition Activities

The project referral did not indicate whether existing structures on the proposed site will be demolished. Demolition activities can have potential negative air quality impacts, including issues surrounding proper handling, demolition, and disposal of asbestos containing material (ACM). Asbestos containing materials could be encountered during demolition or remodeling of existing buildings. Asbestos can also be found in utility pipes/pipelines (transite pipes or insulation on pipes). **If utility pipelines are scheduled for removal or relocation; or building(s) are removed or renovated this project may be subject to various regulatory jurisdictions, including the requirements stipulated in the National Emission Standard for Hazardous Air Pollutants (40CFR61, Subpart M - asbestos NESHA).**

These requirements include but are not limited to: 1) notification requirements to the APCD, 2) asbestos survey conducted by a Certified Asbestos Inspector, and, 3) applicable removal and disposal requirements of identified ACM. Please contact Tim Fuhs of the Enforcement Division at 781-5912 for further information.

#### Permits

Based on the information provided, we are unsure of the types of equipment that may be present at the site. Portable equipment used during construction activities may require California statewide portable equipment registration (issued by the California Air Resources Board) or a District permit. Operational sources, such as back up generators, may also require APCD permits. **To minimize potential delays, prior to the start of the project, please contact David Dixon of the District's Engineering Division at (805) 781-5912 for specific information regarding permitting requirements.**

#### **OPERATIONAL PHASE MITIGATION:**

The APCD staff considered the operational impact of this commercial development by running the URBEMIS 2002 computer model, a tool for estimating vehicle travel, fuel use and the resulting emissions related to this project's land uses. This indicated that operational phase impacts will likely be more than the APCD's CEQA Tier I significance threshold value of 10 lbs/day for nitrogen oxides (NOx), Reactive Organic Gases (ROG) and Particulate Matter (PM10).

#### **Standard Mitigation Measures (Include all standard mitigation measures below)**

- Provide on-site bicycle parking. One bicycle parking space for every 10 car parking spaces is considered appropriate.
- Provide on-site eating, refrigeration and food vending facilities to reduce employee lunchtime trips.
- Provide employee lockers to encourage employees to bike and/or walk to work.

- Increase the building energy efficiency rating by 10% above what is required by Title 24 requirements. This can be accomplished in a number of ways (increasing attic, wall, or floor insulation, installing double pane windows, using efficient interior lighting, etc.).

**Additional Mitigation Measures (Include at least 6 of the following)**

Site Design Mitigation for this Commercial Project

- Increase street shade tree planting.
- Increase shade tree planting in parking lots to reduce evaporative emissions from parked vehicles.
- Provide on-site child care facilities for use by employees.
- Implement on-site circulation design elements in parking lots to reduce vehicle queuing and improve the pedestrian environment with designated walkways.
- Provide pedestrian signage to improve pedestrian safety.

Transportation Demand Mitigation

- If the project is located on an established transit route, improve public transit accessibility by providing a transit turnout with direct pedestrian access to the project or improve existing transit stop amenities.
- Implement the incentive based Transportation Choices Program. The applicant should work with the Transportation Choices Coalition partners to receive free consulting services on how to start and maintain a program. Contact SLO Regional Rideshare at 541-2277.
- Provide Transportation Choices Program information centers on alternative transportation modes at the site (i.e. a transportation kiosk). Contact SLO Regional Rideshare for appropriate materials at 541-2277.

Energy Efficiency Measures

- Use roof material with a solar reflectance value meeting the EPA/DOE Energy Star® rating to reduce summer cooling needs.
- Use low energy parking lot and street lights (e.g. sodium).
- Install door sweeps or weather stripping if more energy efficient doors and windows are not available.
- Install high efficiency or gas space heating.

16. Use and operation of the project and its appurtenances shall be conducted in compliance with the City's General Performance Standards for all uses (Section 21.21.040 of Chapter 21.21 Performance Standards of the City's Zoning Ordinance).

Engineering Conditions:

17. City Standard street improvements shall be constructed on Theatre Drive along the frontage of the project in accordance with the plan line for Theatre Drive adopted by the City Council and plans approved by the City Engineer. An appropriate paving transition shall be extended southerly in accordance with plans approved by the City Engineer. Drainage structures must be designed and constructed as needed.
18. The existing overhead utilities along the east side of Theatre Drive across from the frontage of the project shall be relocated underground.
19. The 10-inch water main in Theatre Drive shall be extended to the south boundary of the project. Domestic lines serving the project shall be metered on the Theatre Drive frontage. The fire line/s double check valve assembly shall be placed on the Theatre Drive frontage in an underground vault or otherwise screened as approved by the City Planner.

20. The 10-inch sewer line in Theatre shall be extended southerly as needed to provide for a private sewer line to be extended from Theatre Drive to serve the project.
21. Storm water detention facilities shall be placed on-site in accordance with City Standards to mitigate the impact of increased volumes of storm water due to development of the site. Storm water quality devices shall be installed in accordance with a Storm Water Pollution Prevention Plan and plans approved by the City Engineer.
22. The applicant shall enter into an agreement to pay a proportionate share of the cost of Highway 46W-101 interchange improvements, as determined by the City, and to participate in the formation of an assessment district for those improvements.

Emergency Services Conditions:

23. Provide fire sprinkler systems for residential, commercial, and industrial buildings.
24. Provide secondary emergency vehicle access sufficient to support the City's fire apparatus (HS-20 Truck Loading). Secondary vehicle access to be at least twenty (20) feet wide with no less than thirteen feet, six inches vertical clearance. All secondary emergency vehicle access surfaces shall provide all weather driving capabilities and conform to the requirements of City Zoning Codes.
25. Prior to the start of construction, documentation shall be submitted to Emergency Services showing that required fire flows can be provided to meet all project demands.

PASSED AND ADOPTED THIS 27<sup>th</sup> day of February, 2007 by the following Roll Call Vote:

AYES: Commissioners –  
 NOES: Commissioners -  
 ABSENT: Commissioners -  
 ABSTAIN: Commissioners -

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CHAIRMAN MARGARET HOLSTINE

ATTEST:

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RON WHISENAND, PLANNING COMMISSION SECRETARY

**EXHIBIT A OF RESOLUTION**

**CITY OF EL PASO DE ROBLES STANDARD DEVELOPMENT CONDITIONS  
FOR PLANNED DEVELOPMENTS / CONDITIONAL USE PERMITS**

PROJECT #: PD 04-024

APPROVING BODY: PLANNING COMMISSION

DATE OF APPROVAL: FEBRUARY 27, 2007

APPLICANT: DURAND-MONSON – WEBB

LOCATION: THEATRE DRIVE/NUTWOOD CIRCLE

The following conditions that have been checked are standard conditions of approval for the above referenced project. The checked conditions shall be complied with in their entirety before the project can be finalized, unless otherwise specifically indicated. In addition, there may be site specific conditions of approval that apply to this project in the resolution.

**COMMUNITY DEVELOPMENT DEPARTMENT - The applicant shall contact the Community Development Department, (805) 237-3970, for compliance with the following conditions:**

**A. GENERAL CONDITIONS:**

- 1. This project approval shall expire on February 27, 2009 (See Planned Development Approval Resolution) unless a time extension request is filed with the Community Development Department prior to expiration.
- 2. The site shall be developed and maintained in accordance with the approved plans and unless specifically provided for through the Planned Development process shall not waive compliance with any sections of the Zoning Code, all other applicable City Ordinances, and applicable Specific Plans.
- 3. Prior to occupancy, all conditions of approval shall be completed to the satisfaction of the City Engineer and Community Developer Director or his designee.
- 4. Any site specific condition imposed by the Planning Commission in approving this project may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the approval of this project. No such modification shall be made unless the Commission finds that such modification is necessary to protect the

(Adopted by Planning Commission Resolution 94-038)



public interest and/or neighboring properties, or, in the case of deletion of an existing condition, that such action is necessary to permit reasonable operation and use for this approval.

- 5. This project is subject to the California Environmental Quality Act (CEQA) which requires the applicant submit a \$25.00 filing fee for the Notice of Determination payable to "County of San Luis Obispo". The fee should be submitted to the Community Development Department within 24 hours of project approval which is then forwarded to the San Luis Obispo County Clerk. Please note that the project may be subject to court challenge unless the required fee is paid.
- 6. The site shall be kept in a neat manner at all times and the landscaping shall be continuously maintained in a healthy and thriving condition.
- 7. All signs shall be subject to review and approval as required by Municipal Code Section 21.19 and shall require a separate application and approval prior to installation of any sign.
- 8. All outdoor storage shall be screened from public view by landscaping and walls or fences per Section 21.21.110 of the Municipal Code.
- 9. All trash enclosures shall be constructed of decorative masonry block compatible with the main buildings. Gates shall be view obscuring and constructed of durable materials such as painted metal or chain link with plastic slatting.
- 10. All existing and/or new ground-mounted appurtenances such as air-conditioning condensers, electrical transformers, backflow devices etc., shall be screened from public view through the use of decorative walls and/or landscaping subject to approval by the Community Development Director or his designee. Details shall be included in the building plans.
- 11. All existing and/or new roof appurtenances such as air-conditioning units, grease hoods, etc. shall be screened from public view. The screening shall be architecturally integrated with the building design and constructed of compatible materials to the satisfaction of the Community Development Director or his designee. Details shall be included in the building plans.
- 12. All existing and/or new lighting shall be shielded so as to be directed downward in such a manner as to not create off-site glare or adversely impact adjacent properties. The style, location and height of the lighting fixtures shall be submitted with the building plans and shall be subject to approval by the Community Development Director or his designee.

- 13. All existing and/or new landscaping shall be installed with automatic irrigation systems.
- 14. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 15. The following areas shall be placed in the Landscape and Lighting District:

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The developer shall install all improvements and landscape areas. City acceptance on behalf of the Landscape and Lighting District shall be subject to the approval of the Public Works Street Department (237-3864).

- 16. All parking lot landscape planters shall have a minimum outside dimension of six feet and shall be separated from parking and driving areas by a six inch high solid concrete curb.
- 17. The following areas shall be permanently maintained by the property owner, Homeowners' Association, or other means acceptable to the City:

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- 18. It is the property owner's responsibility to insure that all construction of private property improvements occur on private property. It is the owner's responsibility to identify the property lines and insure compliance by the owner's agents.

**B. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO THE ISSUANCE OF BUILDING PERMITS:**

- 1. Two sets of the revised Planning Commission approved plans incorporating all Conditions of Approval, standard and site specific, shall be submitted to the Community Development Department prior to the issuance of building permits.

(Adopted by Planning Commission Resolution 94-038)

- 2. Prior to the issuance of building permits, the
  - Development Review Committee shall approve the following:
  - Planning Division Staff shall approve the following:
    - a. A detailed site plan indicating the location of all structures, parking layout, outdoor storage areas, walls, fences and trash enclosures;
    - b. A detailed landscape plan;
    - c. Detailed building elevations of all structures indicating materials, colors, and architectural treatments;
    - d. Other: See PD 00-023 Resolution for specific DRC review requirements.
- 3. The applicant shall meet with the City's Crime Prevention Officer prior to the issuance of building permits for recommendations on security measures to be incorporated into the design of the structures to be constructed. The applicant is encouraged to contact the Police Department at (805) 237-6464 prior to plan check submittal.

**C. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO OCCUPANCY:**

- 1. Occupancy of the facility shall not commence until such time as all Uniform Building Code and Uniform Fire Code regulations have been complied with. Prior to occupancy, plans shall be submitted to the Paso Robles Fire Department and the Building Division to show compliance. The building shall be inspected by the appropriate department prior to occupancy.
- 2. All public or private manufactured slopes located adjacent to public right-of-ways on property in excess of six (6) feet in vertical height and of 2.5:1 or greater slope shall be irrigated and landscaped for erosion control and to soften their appearance as follows: one 15-gallon tree per each 250 square feet of slope area, one 1-gallon or larger size shrub per each 100 square feet of slope area, and appropriate ground cover. Trees and shrubs shall be staggered in clusters to soften and vary the slope plane. Slope planting shall include a permanent irrigation system be installed by the developer prior to occupancy. In lieu of the above planting ratio, the applicant may submit a slope planting plan by a licensed landscape architect or contractor providing adequate landscaping, erosion control and slope retention measures; the slope planting plan is subject to approval by the Development Review Committee. Hydroseeding may be considered on lots of 20,000 square feet or greater.

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(Adopted by Planning Commission Resolution 94-038)

**PUBLIC WORKS DEPARTMENT - The applicant shall contact the Engineering Division, (805) 237-3860, for compliance with the following conditions:**

APPLICANT: DURAND-MONSON PREPARED BY: JF

REPRESENTATIVE: \_\_\_\_\_ CHECKED BY: \_\_\_\_\_

PROJECT: PD 04-024 TO PLANNING: \_\_\_\_\_

All conditions marked are applicable to the above referenced project for the phase indicated.

**D. PRIOR TO ANY PLAN CHECK:**

- 1. The applicant shall enter into an Engineering Plan Check and Inspection Services Agreement with the City.

**E. PRIOR TO ISSUANCE OF A GRADING PERMIT:**

- 1. Prior to approval of a grading plan, the developer shall apply through the City, to FEMA and receive a Letter of Map Amendment (LOMA) issued from FEMA. The developer's engineer shall provide the required supporting data to justify the application.
- 2. The proposed structures and grading shall not encroach into the 100-year floodway as specified in Municipal Code Chapter 21.14 "Flood Damage Prevention Regulations".
- 3. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No. 553, Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to removal.
- 4. A complete grading and drainage plan prepared by a registered civil engineer shall be included with the improvement plans. Drainage calculations shall be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer.

(Adopted by Planning Commission Resolution 94-038)

- 5. A Preliminary Soils and/or Geology Report shall be prepared by a registered engineer for the property to determine the presence of expansive soils or other soils problems and shall make recommendations regarding grading of the proposed site.

**F. PRIOR TO ANY SITE WORK:**

- 1. All off-site public improvement plans shall be prepared by a registered civil engineer and shall be submitted to the City Engineer for review and approval. The improvements shall be designed and placed to the Public Works Department Standards and Specifications.
- 2. The applicant shall submit a composite utility plan signed as approved by a representative of each public utility, together with the improvement plans. The composite utility plan shall also be signed by the Water, Fire, Wastewater, and Street Division heads.
- 3. Any grading anticipated during the rainy season (October 15 to April 15) will require the approval of a Construction Zone Drainage and Erosion Control Plan to prevent damage to adjacent property. Appropriateness of areas shall be subject to City Engineer approval.
- 4. Any construction within an existing street shall require a Traffic Control Plan. The plan shall include any necessary detours, flagging, signing, or road closures requested. Said plan shall be prepared and signed by a registered civil or traffic engineer.
- 5. Landscape and irrigation plans for the public right-of-way shall be incorporated into the improvement plans and shall require a signature of approval by the Department of Public Works, Street Superintendent and the Community Development Department.
- 6. The owner shall offer to dedicate and improve the following street(s) to the standard indicated:

Theatre Drive	Theatre Drive	
Street Name	City Standard	Standard Drawing No.

- 7. The owner shall offer to dedicate to the City the following easement(s). The location and alignment of the easement(s) shall be to the description and satisfaction of the City Engineer:
  - a. Public Utilities Easement;

(Adopted by Planning Commission Resolution 94-038)

- b. Water Line Easement;
- c. Sewer Facilities Easement;
- d. Landscape Easement;
- e. Storm Drain Easement.

**G. PRIOR TO ISSUANCE OF A BUILDING PERMIT:**

- 1. A final soils report shall be submitted to the City prior to the final inspection and shall certify that all grading was inspected and approved, and that all work has been done in accordance with the plans, preliminary report, and Chapter 70 of the Uniform Building Code.
- 2. The applicants civil and soils engineer shall submit a certification that the rough grading work has been completed in substantial conformance to the approved plans and permit.
- 3. When retaining walls are shown on the grading plan, said walls shall be completed before approval of the rough grade, and prior to issuance of any building permits, unless waived by the Building Official and the City Engineer.
- 4. All property corners shall be staked for construction control, and shall be promptly replaced if destroyed.
- 5. Building permits shall not be issued until the water system has been completed and approved, and a based access road installed sufficient to support the City's fire trucks per Fire Department recommendation.
- 6. The developer shall annex to the City's Landscape and Lighting District for payment of the operating and maintenance costs of the following:
  - a. Street lights;
  - b. Parkway and open space landscaping;
  - c. Wall maintenance in conjunction with landscaping;
  - d. Graffiti abatement;
  - e. Maintenance of open space areas.
- 7. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate Map (FIRM) - in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V - the developer shall provide an Elevation Certificate in accordance with the National Flood Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State of California.
- 8. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate

(Adopted by Planning Commission Resolution 94-038)

Map (FIRM) in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V, the developer shall provide a Flood Proofing Certificate in accordance with the National Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State California.

**H. PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY:**

- 1. The applicant shall pay any current and outstanding fees for Engineering Plan Checking and Construction Inspection services and any outstanding annexation fees.
- 2. No buildings shall be occupied until all public improvements are completed and approved by the City Engineer, and accepted by the City Council.
- 3. All final property corners and street monuments shall be installed before acceptance of the public improvements.
- 4. All top soil removed shall be stockpiled and evenly distributed over the slopes and lots upon completion of rough grading to support hydroseeding and landscaping. All slope areas shall be protected against erosion by hydroseeding or landscaping.
- 5. The applicant shall install all street names, traffic signs and traffic striping as directed by the City Engineer.
- 6. If the adjoining existing City street is inadequate for the traffic generated by the project, or will be severely damaged by the construction, the applicant shall remove the entire roadway and replace it with a minimum full half-width street plus a 12' wide travel lane and 8' wide graded shoulder adequate to provide for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 7. If the development includes a phased street construction along the project boundary for future completion by the adjacent property owner, the applicant shall provide a minimum half-width street plus a 12' wide travel lane and 4' wide graded shoulder adequate for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 8. When the project fronts on an existing street, the applicant shall pave-out from the proposed curb to the edge of pavement if the existing pavement section is adequate, and shall feather the new paving out to the centerline for a smooth transition. If the existing pavement is inadequate, the roadway shall be replaced to centerline and the remaining pavement shall be overlaid. (A finding of "rough proportionality" has been made in the resolution for this condition).

(Adopted by Planning Commission Resolution 94-038)

- ☒ 9. Any utility trenching in existing streets shall be overlaid to restore a smooth riding surface as required by the City Engineer. Boring and jacking rather than trenching may be required on newly constructed or heavily traveled City streets.
- ☒ 10. The applicant shall install all utilities (sewer, water, gas, electricity, cable TV and telephone) underground (as shown on the composite utility plan). Street lights shall be installed at locations as required by the City Engineer. All existing overhead utilities adjacent to or within the project shall be relocated underground except for electrical lines 77 kilovolts or greater. All utilities shall be extended to the boundaries of the project. All underground construction shall be completed and approved by the public utility companies, and the subgrade shall be scarified and compacted, before paving the streets.
- ☒ 11. Prior to paving any street the water and sewer systems shall successfully pass a pressure test. The sewer system shall also be tested by a means of a mandrel and video inspection with a copy of the video tape provided to the City. No paving shall occur until the City has reviewed and viewed the sewer video tape and has determined that the sewerline is acceptable. Any repair costs to the pipeline including trench paving restoration shall be at the developer's expense.
- ☒ 12. A blackline clear Mylar (0.4 MIL) copy and a blueline print of as-built improvement plans, signed by the engineer of record, shall be provided to the City Engineer prior to the final inspection. A reduced copy (i.e. 1" = 100') of the composite utility plan shall be provided to update the City's Atlas Map.
- ☒ 13. All construction refuse shall be separated (i.e. concrete, asphalt concrete, wood gypsum board, etc.) and removed from the project in accordance with the City's Source Reduction and Recycling Element.

\*\*\*\*\*



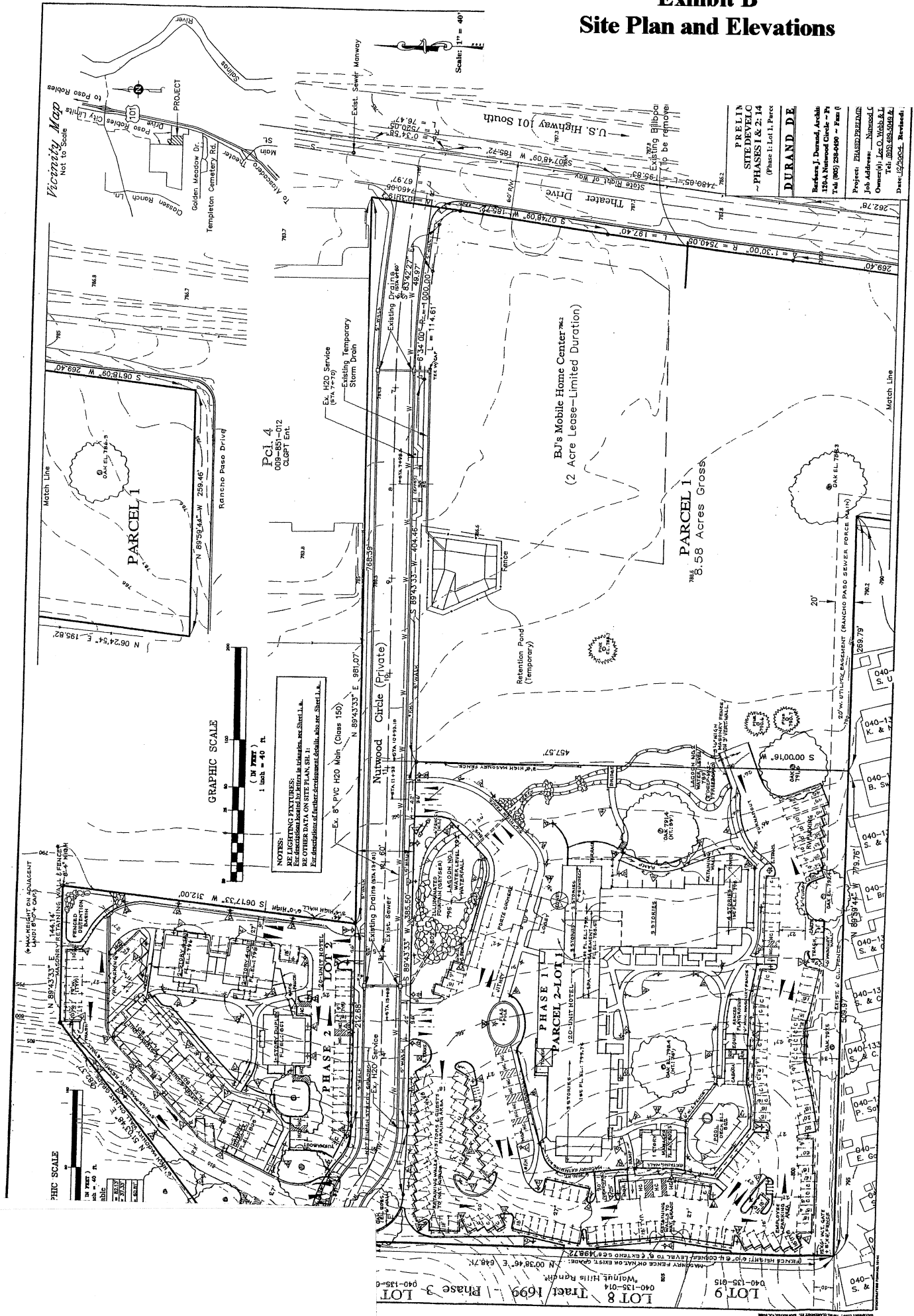
**PASO ROBLES FIRE DEPARTMENT - The applicant shall contact the Fire Department, (805) 237-3973, for compliance with the following conditions:**

**I. GENERAL CONDITIONS**

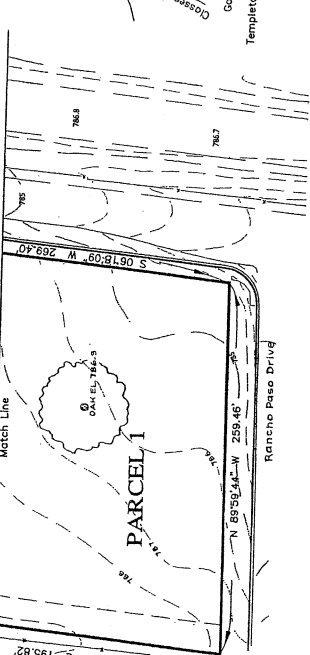
- 1. Fire hydrants shall be installed at intervals as required by the Fire Chief and City Engineer. The maximum spacing for single family residential shall be 500 feet. The maximum spacing for multi-family and commercial/ residential shall be 300 feet. On-site hydrants shall be placed as required by the Fire Chief.
- 2. Building permits shall not be issued until the water system, including hydrants, has been tested and accepted and a based access road installed sufficient to support the City's fire apparatus (HS-20 truck loading). The access road shall be kept clear to a minimum of 24 feet at all times and shall be extended to each lot and shall be maintained to provide all weather driving conditions.
- 3. No buildings shall be occupied until all improvements are completed and accepted by the City for maintenance.
- 4. If the development includes phased street construction, temporary turn-arounds shall be provided for streets that exceed 150 feet in length. The temporary turn around shall meet City requirements as set forth in the Public Works Department Standards and Specifications.
- 5. All open space areas to be dedicated to the City shall be inspected by the Fire Department prior to acceptance. A report shall be submitted recommending action needed for debris, brush and weed removal and tree trimming. The developer shall clean out all debris, dead limbs and trash from areas to be recorded as open space prior to acceptance into a Benefit Maintenance District.
- 6. Any open space included in a private development shall be subject to the approval of a vegetation management plan approved by the Fire Chief.
- 7. Each tract or phase shall provide two sources of water and two points of access unless otherwise determined by the Fire Chief and Public Works Director.
- 8. Provisions shall be made to update the Fire Department Run Book.

(Adopted by Planning Commission Resolution 94-038)

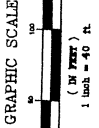
# Exhibit B Site Plan and Elevations



*Vicinity Map*  
Not to Scale



Parcel 4  
009-851-012  
CLEPT Ent.



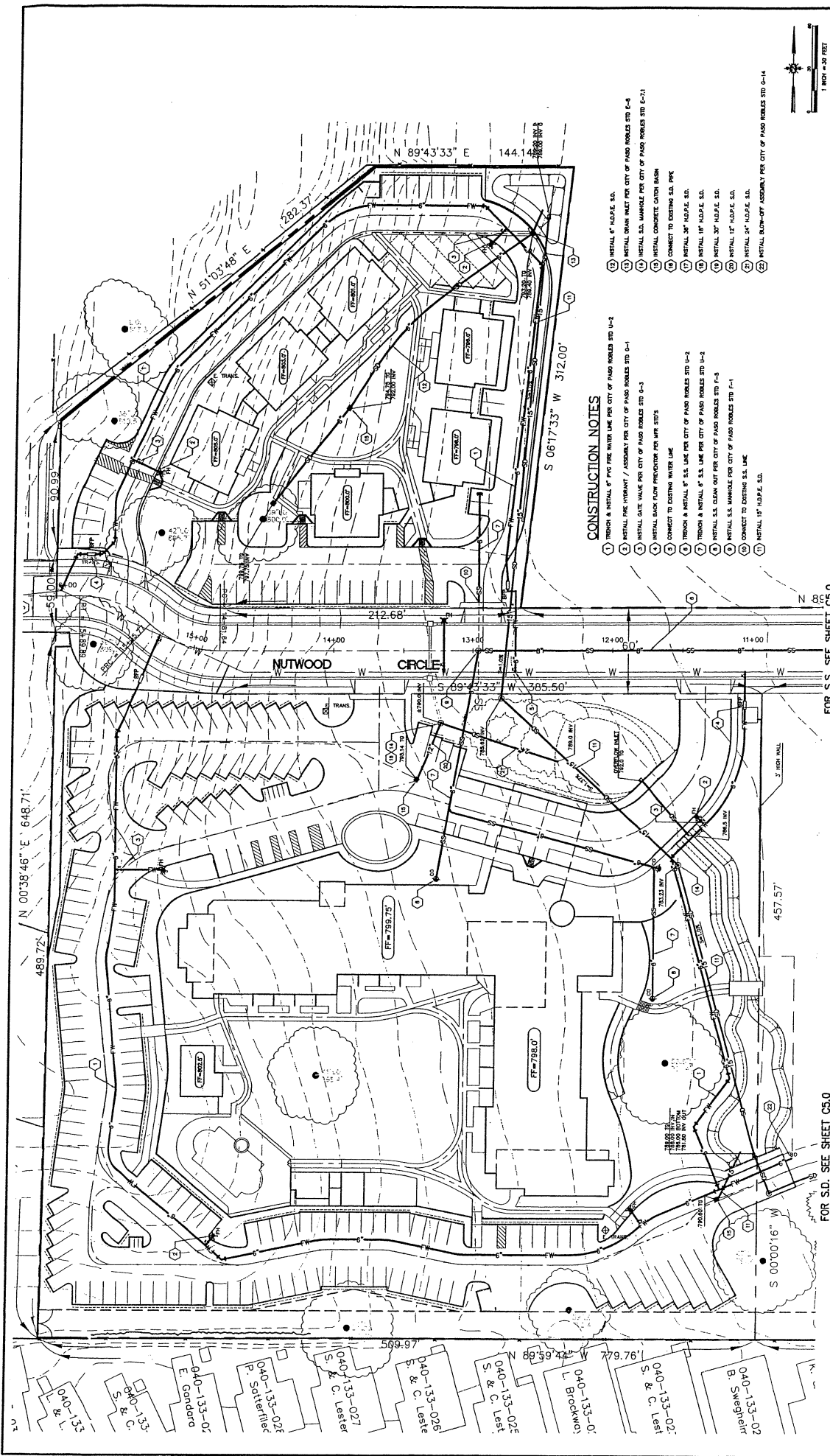
NOTES:  
1. EXISTING UTILITY LINES, EXCEPT FOR THE EXISTING H2O SERVICE LINE, ARE SHOWN AS DASHED LINES. SEE SHEET L.L. FOR OTHER DATA ON SITE PLAN, SH. 1.  
2. DESCRIPTIONS OF UTILITIES AND ELEVATIONS, ALSO SEE SHEET L.L.

PRELIM SITE DEVELOPMENT PHASES I & 2: 14 (Phase I: Lot 1, Phase II: Lot 2)

**DURAND DE**

Barbara J. Durand, Architect  
12344 Nutwood Circle - Paso Robles, CA 92371  
Tel: (805) 238-6496 - Fax: (805) 238-6497

Project: PHASE I PRELIM  
Job Address: Nutwood Circle  
Owner(s): L.C.O. Webb & L. Webb  
Tel: (805) 238-6496  
Date: 12/20/04 - Revised

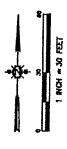


**CONSTRUCTION NOTES**

- 1 TRENCH & INSTALL 6" PVC WATER LINE PER CITY OF PASO ROBLES STD U-2
- 2 INSTALL FIRE HYDRANT / ASSEMBLY PER CITY OF PASO ROBLES STD U-1
- 3 INSTALL GATE VALVE PER CITY OF PASO ROBLES STD U-3
- 4 INSTALL BACK FLOW PREVENTER PER MFR STOPS
- 5 TRENCH & INSTALL 4" S.S. LINE PER CITY OF PASO ROBLES STD U-2
- 6 TRENCH & INSTALL 6" S.S. LINE PER CITY OF PASO ROBLES STD U-2
- 7 INSTALL S.S. CLEAN OUT PER CITY OF PASO ROBLES STD P-3
- 8 INSTALL S.S. WASTE PER CITY OF PASO ROBLES STD T-1
- 9 CONNECT TO EXISTING S.S. LINE
- 10 INSTALL 15" H.O.P.E. S.S.
- 11 INSTALL 6" H.O.P.E. S.S.
- 12 INSTALL DRAIN W/LET PER CITY OF PASO ROBLES STD E-4
- 13 INSTALL S.S. WASTE PER CITY OF PASO ROBLES STD E-7-1
- 14 INSTALL CONCRETE CATCH BASIN
- 15 CONNECT TO EXISTING S.S. PIPE
- 16 INSTALL 30" H.O.P.E. S.S.
- 17 INSTALL 18" H.O.P.E. S.S.
- 18 INSTALL 30" H.O.P.E. S.S.
- 19 INSTALL 12" H.O.P.E. S.S.
- 20 INSTALL 30" H.O.P.E. S.S.
- 21 INSTALL 24" H.O.P.E. S.S.
- 22 INSTALL BLOW-OFF ASSEMBLY PER CITY OF PASO ROBLES STD U-4

FOR S.S. SEE SHEET C5.0

FOR S.D. SEE SHEET C5.0



**REVISIONS**

NO.	DATE	DESCRIPTION

**WEBB & MONSON HOTEL BY DURAND DESIGNS**  
**PRELIMINARY UTILITY PLAN**  
**CITY OF PASO DE ROBLES, CA.**

**RTG**  
**R THOMPSON CONSULTING, INC.**  
 CIVIL ENGINEERING  
 7000 MONROE ROAD  
 PASO ROBLES, CA 93271  
 (559) 255-1100

DESIGNED BY: [Signature]  
 CHECKED BY: [Signature]  
 DATE: JUL 05 2005

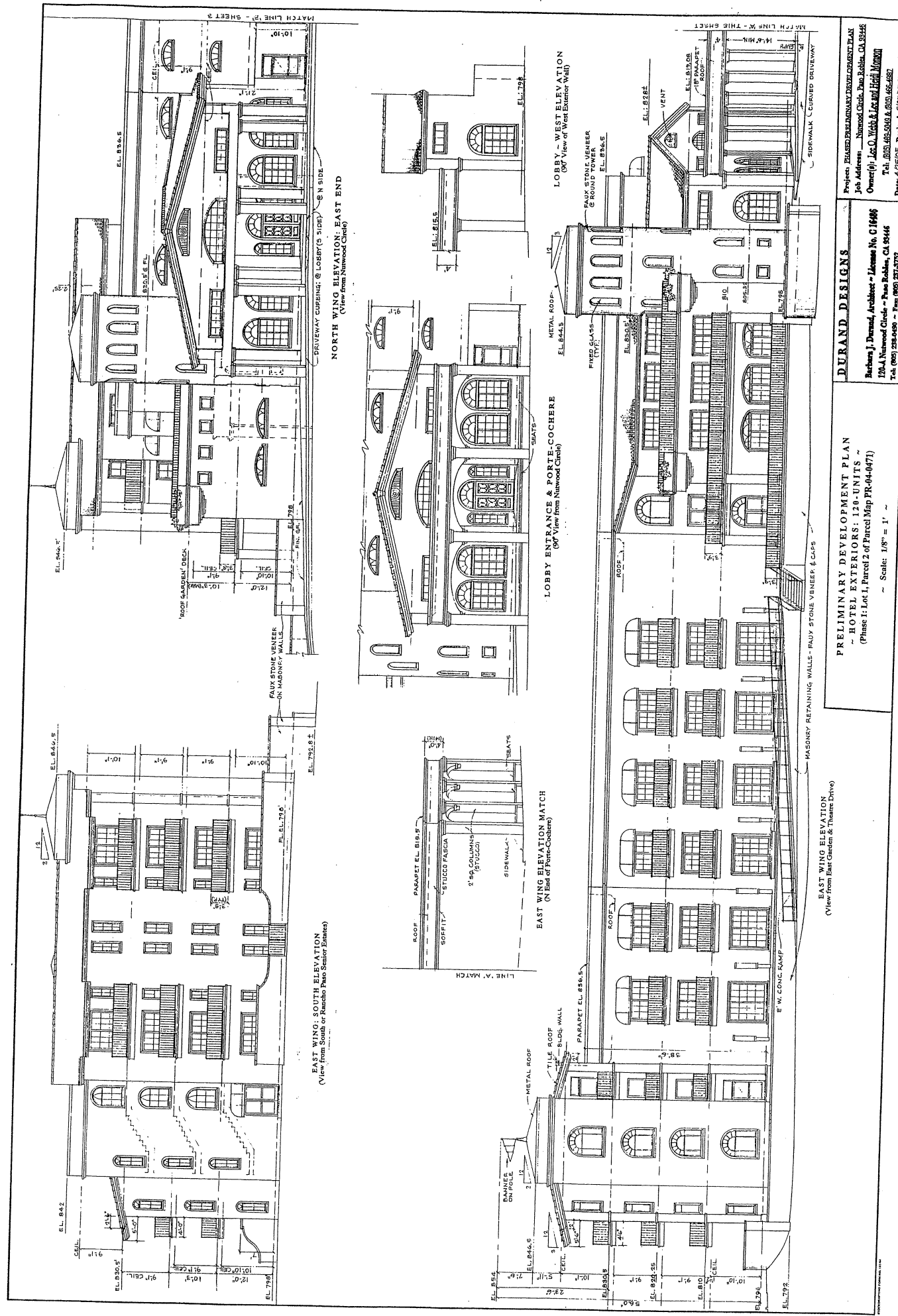
PROJECT NO: C40  
 SHEET NO: 4 OF 5

These plans and specifications, and the laws and orders incorporated herein, are the property of R Thompson Consulting, Inc. and shall remain the property of R Thompson Consulting, Inc. without authority of the City of Paso Robles. If these plans are used for any other project, the user assumes all liability for any errors or omissions. R Thompson Consulting, Inc. is not responsible for any errors or omissions on these plans.

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**REGISTERED PROFESSIONAL ENGINEER**  
**THOMPSON CONSULTING, INC.**  
 CIVIL  
 STATE OF CALIFORNIA

- 040-133-01 S. & C. Lester
- 040-133-02 E. Gondro
- 040-133-03 P. Satterfield
- 040-133-04 S. & C. Lester
- 040-133-05 S. & C. Lester
- 040-133-06 S. & C. Lester
- 040-133-07 L. Brockway
- 040-133-08 S. & C. Lester
- 040-133-09 B. Sieweglein



**DURAND DESIGNS**  
 Project PHASED PRELIMINARY DEVELOPMENT PLAN  
 Job Address: N.wood Circle, Paso Robles, CA 92546  
 Owner: (p) L.O. Walsh & Associates High Mobility  
 Tel: (805) 465-5069 & (805) 665-4887  
 Fax: (805) 665-5069

**DURAND DESIGNS**  
 Barbara J. Durand, Architect - License No. C 16446  
 1244 N.wood Circle - Paso Robles, CA 92546  
 Tel: (805) 238-6400 - Fax: (805) 237-5752

**PRELIMINARY DEVELOPMENT PLAN**  
 HOTEL EXTERIORS: 120 - UNITS  
 (Phase I: Lot 1, Parcel 2 of Parcel Map PR-04-0471)  
 Scale: 1/8" = 1' ~

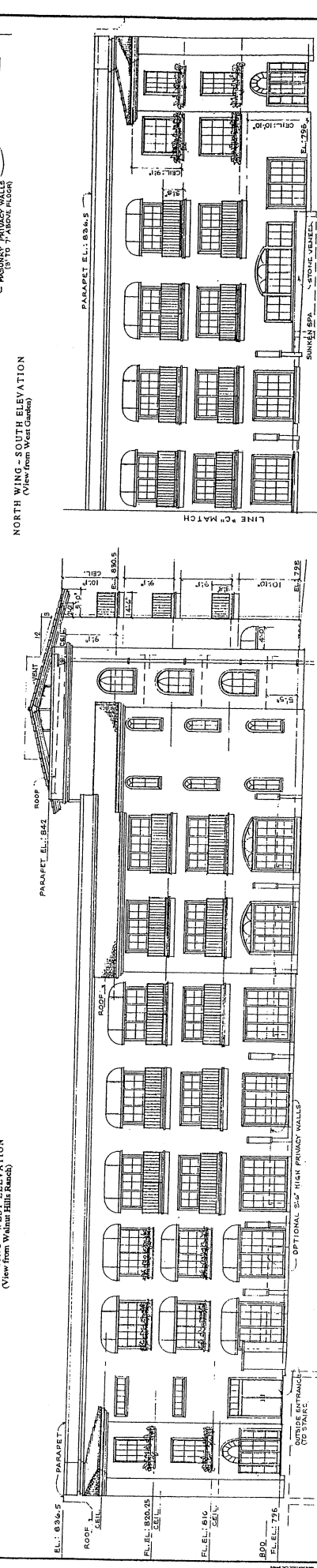
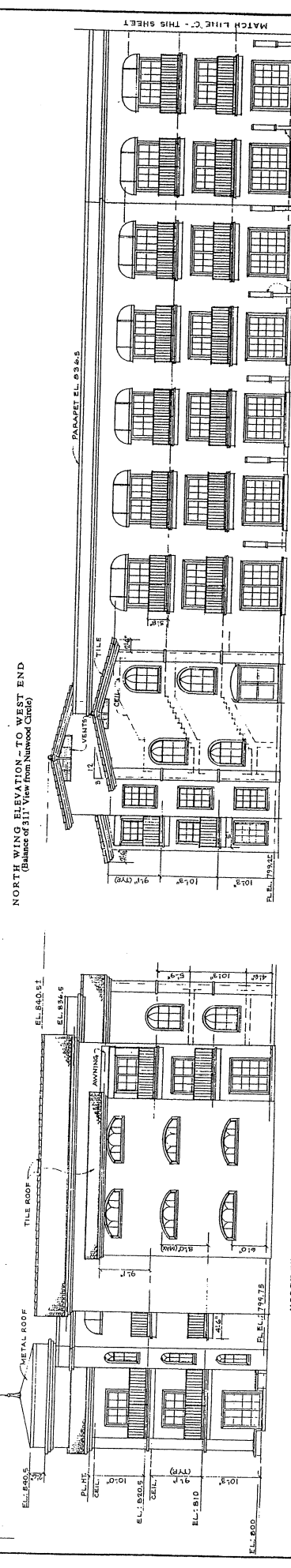
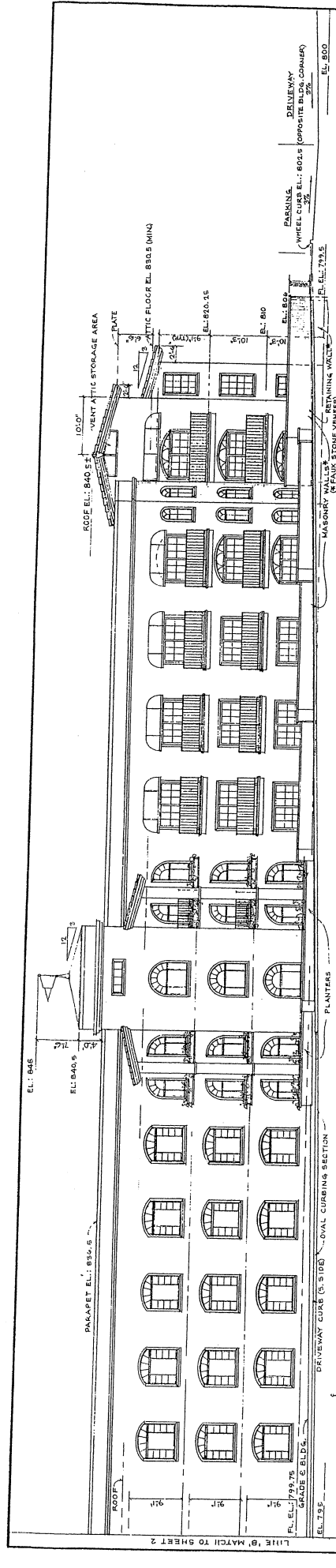
EAST WING ELEVATION  
 (View from East Garden & Future Drive)

LOBBY ENTRANCE & PORTE-COCHERE  
 (S View from N.wood Circle)

LOBBY, WEST ELEVATION  
 (S View of West Exterior Wall)

NORTH WING ELEVATION: EAST END  
 (View from N.wood Circle)

EAST WING: SOUTH ELEVATION  
 (View from South or Rancho Paso Senior Estates)



**PRELIMINARY DEVELOPMENT PLAN**  
 - HOTEL EXTERIORS: 120-UNITS -  
 (Phase 1: Lot 1, Parcel 2 of parcel Map PR-04-0071)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**  
 Project: PHASED PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Narewood Circle, Paso Robles, CA 93446  
 Owner(s): Lico, Wash. & Lico and Heidi, Kesson  
 Tel: 800.485.5443 & 800.462.4387  
 Date: 6/21/05 Revised: 8/24/05 Sheet 2 of 5

NORTH WING - WEST ELEVATION  
 (View from West Garden)

NORTH WING - SOUTH ELEVATION  
 (View from West Garden)

EAST WING - WEST ELEVATION  
 (View from West Garden)

NORTH WING - WEST ELEVATION TO WEST END  
 (Balance of 311, View from Narewood Circle)

NORTH WING - WEST ELEVATION  
 (View from Walnut Hills Branch)

**MANAGER'S COTTAGE**  
 (View from West Garden)  
 (North View)  
 (South View - Patio)  
 (Rear View)

**ADDITIONAL STRUCTURES - LOT 1, PARCEL 2**  
 CABANA - West View  
 CABANA - North View

**FOUR-PLEX COTTAGE (16 of 20 units; Lot 2)**  
 (Typical Rear View)

**FOUR-PLEX COTTAGE (16 of 20 units; Lot 2)**  
 (Typical Left Side View)

**FOUR-PLEX COTTAGE (16 of 20 units; Lot 2)**  
 (Typical Front View - Bottom stairs may be reversed)

**DUPLEX COTTAGE (4 of 20 Units; Lot 2)**  
 (Typical Front View)

**DUPLEX COTTAGE (4 of 20 Units; Lot 2)**  
 (Typical Rear View - Plus Entry Wall at Ninnwood Circle)

**DUPLEX COTTAGE (4 of 20 Units; Lot 2)**  
 (Right Side Patio View - Reverse for Left Side View)

**PERIMETER FENCING & RETAINING WALL MATERIALS**

**PRELIMINARY DEVELOPMENT PLAN**  
 - HOTEL EXTERIORS -  
 MANAGER'S UNIT - 20 COTTAGE SUITES  
 (Phases 1 and 2 of PD 04-024)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**  
 Project: PHASED PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Ninnwood Circle, Pine Robbin, GA 35466  
 Owner(s): Lee O. West & Lee and Michelle Linneman  
 Tel: (803) 468-5469 / (803) 465-5971  
 Fax: (803) 254-4400 ~ Fax (803) 257-8732

PREPARED BY: DURAND DESIGNS, INC. 10/10/04

PROOF OF PUBLICATION

LEGAL NEWSPAPER NOTICES

PLANNING COMMISSION/CITY COUNCIL  
PROJECT NOTICING

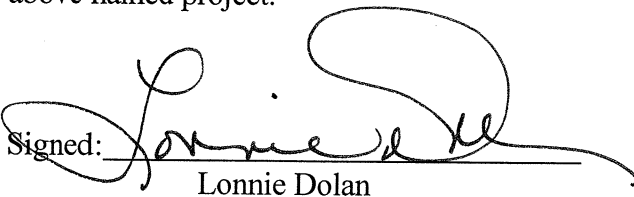
Newspaper: Tribune

Date of Publication: February 7, 2007

Meeting Date: February 27, 2007  
(Planning Commission)

Project: Planned Development 04-024  
(Webb & Monson-Theatre Dr.)

I, Lonnie Dolan, employee of the Community Development Department, Planning Division, of the City of El Paso de Robles, do hereby certify that this notice is a true copy of a published legal newspaper notice for the above named project.

Signed:   
Lonnie Dolan

forms/newsaffi.691

CITY OF EL PASO DE ROBLES

NOTICE OF PUBLIC HEARING

NOTICE OF INTENT TO CONSIDER A  
PLANNED DEVELOPMENT AND TO  
ADOPT A NEGATIVE DECLARATION

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of El Paso de Robles will hold a Public Hearing on Tuesday, February 27, 2007, at 7:30 p.m. at the City of El Paso de Robles, 1000 Spring Street, Paso Robles, California, in the City Council Chambers, to consider a Planned Development and adoption of a Negative Declaration in accordance with the provisions of the California Environmental Quality Act (CEQA) for the following project:

Planned Development 04-024: A request filed by Lee Webb and Lee Monson, to construct a 120 room hotel and 20 cottage/hotel units at 2805 Theatre Drive. (APNs 009-851-023 and -016). The project is in the Highway Commercial Planned Development (C-2 PD) zoning district.

The public review period for this project is February 7th through February 27th, 2007. The proposed project and Negative Declaration may be reviewed at the Community Development Department, 1000 Spring Street, Paso Robles, California. Copies may be purchased for the cost of reproduction.

Written comments on the proposed Planned Development and corresponding Negative Declaration may be mailed to the Community Development Department, 1000 Spring Street, Paso Robles, CA 93446, provided that the comments are received prior to the time of the public hearing. Oral comments may be made at the hearing. Should you have any questions regarding this application, please call Susan DeCarli at (805) 237-3970.

If you challenge the Planned Development or Negative Declaration application in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning Commission at or prior to the public hearing.

Susan DeCarli, AICP  
City Planner  
February 7, 2007

6532492

**AFFIDAVIT  
OF MAIL NOTICES**

**PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING**

I, Susan DeCarli, employee of the City of El Paso de Robles, California, do hereby certify that the mail notices have been processed as required for project Planned Development 04-024  
Mailed on this 16th day of February, 2007.

City of El Paso de Robles  
Community Development Department  
Planning Division

Signed:   
Susan DeCarli



# CITY OF PASO ROBLES – PLANNING DIVISION INITIAL STUDY

## 1. GENERAL PROJECT INFORMATION

<b>PROJECT TITLE:</b>	<b>Durand Hotel and Cottages - PD 04-024</b>
<b>LEAD AGENCY:</b>	City of Paso Robles - 1000 Spring Street, Paso Robles, CA 93446
<b>Contact:</b>	Susan DeCarli, AICP, City Planner
<b>Telephone:</b>	(805) 237-3970
<b>PROJECT LOCATION:</b>	2805 Theatre Drive (APN 009-851-023 and -016)
<b>PROJECT PROPONENT:</b>	Applicant: Lee Monson and Lee Webb 10519 Colorado Road, Atascadero, CA 93422
<b>LEAD AGENCY CONTACT/ INITIAL STUDY PREPARED BY:</b>	Susan DeCarli, AICP, City Planner
<b>Telephone:</b>	(805) 237-3970
<b>Facsimile:</b>	(805) 237-3904
<b>E-Mail:</b>	sdecarli@prcity.com
<b>GENERAL PLAN DESIGNATION:</b>	Regional Commercial (RC)
<b>ZONING:</b>	Highway Commercial – Planned Development (C2-PD)

## 2. PROJECT DESCRIPTION

The proposed project is a request for a Planned Development application for a 120 room resort hotel and 20 cottages. There are two properties involved with this project, which are accessed from a private street, Nutwood Circle.

Surrounding land uses: Land uses to the north include commercial retail and service uses. Properties to the west include single family residences. A senior mobile home park is located to the south of the hotel site. Property located to the east is used as commercial retail on the north side of Nutwood Circle, and is there is a temporary use of mobile home sales on the south side of Nutwood Circle. The properties proposed for development are currently undeveloped.

The 120 unit hotel is proposed on the south side of Nutwood Circle, and the 20 cottages are proposed across the street to the north. A loop driveway is proposed on both properties for site access and circulation, and for the parking lots. The hotel site includes a central open space area, pool and other on-site amenities. The hotel is proposed to include meeting rooms, guest breakfast services, accessory gift shops and a wine tour service desk. An on-site caretakers unit is proposed on the hotel site. The cottages are intended to accommodate guests that would prefer an extended stay, therefore the cottages are proposed to be designed as suites.

There are several oak trees located on the hotel site, which are proposed to be protected within the development. An Arborist Report was prepared for this project, which includes tree protection measures. Most drainage that would result from the project is designed to be directed into landscape areas and water features. The applicant will be required to extend utility lines for City water and sewer services from the project sites to City utility lines in Theatre Drive. A traffic impact study was prepared for this project. The study evaluated project trip generation and impacts on surrounding circulation facilities, including the interchange of Highways 101 and 46 West. Since this project is proposed within the impact area of the interchange, the applicant will be required to participate in the interchange assessment district established for projects in the impact area. Further discussion of traffic impacts and mitigation measures is provided in the Transportation/Circulation Analysis of this Initial Study.

**3. OTHER AGENCIES WHOSE APPROVAL MAY BE REQUIRED (For example, issuance of permits, financing approval, or participation agreement):**

None.

**4. EARLIER ENVIRONMENTAL ANALYSIS AND RELATED ENVIRONMENTAL DOCUMENTATION:**

This Initial Study incorporates by reference the City of El Paso de Robles General Plan Environmental Impact Report (EIR) (SCH#2003011123).

**5. CONTEXT OF ENVIRONMENTAL ANALYSIS FOR THE PROJECT:**

This Initial Study relies on expert opinion supported by the facts, technical studies, and technical appendices of the City of El Paso de Robles General Plan EIR. These documents are incorporated herein by reference. They provide substantial evidence to document the basis upon which the City has arrived at its environmental determination regarding various resources.

**6. PURPOSES OF AN INITIAL STUDY**

The purposes of an Initial Study for a Development Project Application are:

- A. To provide the City with sufficient information and analysis to use as the basis for deciding whether to prepare an Environmental Impact Report, a Mitigated Negative Declaration, or a Negative Declaration for a site specific development project proposal;
- B. To enable the Applicant of a site specific development project proposal or the City as the lead agency to modify a project, mitigating adverse impacts before an Environmental Impact Report is required to be prepared, thereby enabling the proposed Project to qualify for issuance of a Negative Declaration or a Mitigated Negative Declaration;
- C. To facilitate environmental assessment early in the design of a project;
- D. To eliminate unnecessary EIRs;
- E. To explain the reasons for determining that potentially significant effects would not be significant;
- F. To determine if a previously prepared EIR could be used for the project;
- G. To assist in the preparation of an Environmental Impact Report if one is required; and

- H. To provide documentation of the factual basis for the finding of no significant effect as set forth in a Negative Declaration or a Mitigated Negative Declaration prepared for the a project.

## **7. EXPLANATION OF ANSWERS FOUND ON THE ENVIRONMENTAL CHECKLIST FORM**

### **A. Scope of Environmental Review**

This Initial Study evaluates potential impacts identified in the following checklist.

### **B. Evaluation of Environmental Impacts**

1. A brief explanation is required for all answers to the questions presented on the following Environmental Checklist Form, except where the answer is that the proposed project will have “No Impact.” The “No Impact” answers are to be adequately supported by the information sources cited in the parentheses following each question or as otherwise explained in the introductory remarks. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors and/or general standards. The basis for the “No Impact” answers on the following Environmental Checklist Form is explained in further detail in this Initial Study in Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 10 (Context of Environmental Analysis for the Project).
2. All answers on the following Environmental Checklist Form must take into account the whole action involved with the project, including implementation. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more “Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.
4. “Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level. Mitigation Measures from Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). See Section 4 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 11 (Earlier Analysis and Background Materials) of this Initial Study.
6. References to the information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the Environmental Checklist Form. See Section 11 (Earlier Analysis and Related Environmental Documentation). Other sources used or individuals contacted are cited where appropriate.
7. The following Environmental Checklist Form generally is the same as the one contained in Title 14, California Code of Regulations; with some modifications to reflect the City’s needs and requirements.
8. Standard Conditions of Approval: The City imposes standard conditions of approval on Projects. These conditions are considered to be components of and/or modifications to the Project and some reduce or

minimize environmental impacts to a level of insignificance. Because they are considered part of the Project, they have not been identified as mitigation measures. For the readers' information, the standard conditions identified in this Initial Study are available for review at the Community Development Department.

9. Certification Statement: The statements made in this Initial Study and those made in the documents referenced herein present the data and information that are required to satisfy the provisions of the California Environmental Quality Act (CEQA) – Statutes and Guidelines, as well as the City's Procedures for Implementing CEQA. Further, the facts, statements, information, and analysis presented are true and correct in accordance with standard business practices of qualified professionals with expertise in the development review process, including building, planning, and engineering.

**8. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The proposed project may potentially affect the environmental factors checked below, and may involve at least one impact that is a “Potentially Significant Impact” or is “Potentially Significant Unless Mitigated,” if so indicated on the following Environmental Checklist Form (Pages 8 to.15)

- Land Use & Planning       Transportation/Circulation       Public Services
- Population & Housing       Biological Resources       Utilities & Service Systems
- Geological Problems       Energy & Mineral Resources       Aesthetics
- Water       Hazards       Cultural Resources
- Air Quality       Noise       Recreation
- Mandatory Findings of Significance

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**9. ENVIRONMENTAL DETERMINATION:** On the basis of this initial evaluation: I find that:

The proposed project could not have a significant effect on the environment; and, therefore, a **NEGATIVE DECLARATION** will be prepared.

Although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. Therefore, a **MITIGATED NEGATIVE DECLARATION** will be prepared.

The proposed project may have a significant effect on the environment; and, therefore an **ENVIRONMENTAL IMPACT REPORT** is required.

The proposed project may have a significant effect(s) on the environment, but one or more effects (1) have been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) have been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a “potentially significant impact” or is “potentially significant unless mitigated.”

Therefore, an **ENVIRONMENTAL IMPACT REPORT** is required, but it will analyze only the effect or effects that remain to be addressed.

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

February 7, 2007

\_\_\_\_\_  
 Susan DeCarli, AICP, City Planner

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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**I. LAND USE AND PLANNING.** Would the Proposal:

- a) Conflict with general plan designation or zoning? (Sources: 1 & 8)

*Discussion: The proposed project is consistent with the intent of the Regional Commercial land use designation of the General Plan and the Highway Commercial zoning district since it is a proposal for a regionally serving tourist destination hotel development.*

- b) Conflict with applicable environmental plans or policies adopted by agencies with jurisdiction over the project? (Sources: 1 & 3)

*Discussion: The proposed project complies with the EIR recently certified for the City General Plan Update, 2003 and there are no other agencies with jurisdiction over this project.*

- c) Be incompatible with existing land uses in the vicinity? (Sources: 1 & 3)

*Discussion: The surrounding uses include a mix of commercial and residential uses. The proposed hotel and cottage uses would provide an appropriate transition from the existing residential uses to the commercial uses. Dense landscaping along the southern property line should be incorporated into the hotel project and western property line for the cottages, to provide additional buffering between the hotel uses and adjacent residences. Also, as required in the Zoning Code, all outdoor lighting will need to be shielded and downcast to reduce light exposure to adjacent properties from the proposed project. The building footprint of the hotel are proposed to be setback at least 100 feet from the southern property line, which will reduce lighting impacts to surrounding properties.*

- d) Affect agricultural resources or operations (e.g., impacts to soils or farmlands, or impacts from incompatible uses)?

*Discussion: The project site is an urban infill property with no agricultural uses, resources or operations on the property.*

- e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)? (Sources: 1 & 3)

*Discussion: The project is proposed on an infill lot and will not disrupt or divide the established community.*

**II. POPULATION AND HOUSING.** Would the proposal:

- a) Cumulatively exceed official regional or local population projections? (Sources: 1 & 3)

*Discussion: The proposed project is consistent with the build-out alternative planned for and evaluated in the General Plan Update, 2003 and EIR. It is not large enough to result in creating a significant cumulative population growth impacts.*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? (Sources: 1 & 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: This is an existing infill property. The project will not extend major infrastructure that would induce substantial growth since there are existing services and infrastructure surrounding the property to serve the proposed project.</i>				
c) Displace existing housing, especially affordable housing? (Sources: 1, 3, & 5)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The properties are currently vacant, therefore, the project will not displace existing housing.</i>				

**III. GEOLOGIC PROBLEMS.** Would the proposal result in or expose people to potential impacts involving:

- a) Fault rupture? (Sources: 1, 2, & 3)

*Discussion: The potential for and mitigation of impacts that may result from fault rupture in the project area are identified and addressed in the General Plan EIR, pg. 4.5-8. There are two known fault zones on either side of this valley. The Rinconada Fault system runs on the west side of the valley. The San Andreas Fault is on the east side of the valley and runs through the community of Parkfield east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the Uniform Building Code to all new development within the City. Review of available information and examinations indicate that neither of these faults is active with respect to ground rupture in Paso Robles. Soils reports and structural engineering in accordance with local seismic influences would be applied in conjunction with any new development proposal. Based on standard conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant. In addition, per requirements of the Alquist-Priolo Earthquake Fault Zones, only structures for human habitation need to be setback a minimum of 50 feet of a known active trace fault.*

- b) Seismic ground shaking? (Sources: 1, 2, & 3)

*Discussion: The City is located within an active earthquake area that could experience seismic ground shaking from the Rinconada and San Andreas Faults. The proposed structure will be constructed to current UBC codes. The General Plan EIR identified impacts resulting from ground shaking as less than significant and provided mitigation measures that will be incorporated into the design of this project including adequate structural design and not constructing over active or potentially active faults.*

- c) Seismic ground failure, including liquefaction? (Sources: 1, 2 & 3)

*Discussion: Per the General Plan EIR, the project site is located in an area with soil conditions that have a potential for liquefaction or other type of ground failure due to seismic events due to soil conditions. The EIR identifies measures to reduce this potential impact, which will be incorporated into this project. This includes a requirement to conduct a site-specific analysis of liquefaction potential. Based on analysis results, the project design and construction will include specific design requirements to reduce the potential impacts on structures due to liquefaction to a less than significant level.*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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d) Seiche, tsunami, or volcanic hazard? (Sources: 1, 2, & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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e) Landslides or Mudflows? (Sources: 1, 2, & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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*Discussion: d. and e. The project site is not located near bodies of water or volcanic hazards, nor is the site located in an area subject to landslides or mudflows.*

f) Erosion, changes in topography or unstable soil conditions from excavation, grading, or fill? (Sources: 1, 2, 3, & 4)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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*Discussion: Per the General Plan EIR the soil condition is not erosive or otherwise unstable. As such, no significant impacts are anticipated. The site is relatively flat and will need minimal grading.*

g) Subsidence of the land? (Sources: 1, 2, & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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*Discussion: See Item c.*

h) Expansive soils? (Sources: 4)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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*Discussion: Per the General Plan EIR, Paso Robles is an area that has moderately expansive soils. This issue will be addressed through implementation of appropriate soil preparation as determined necessary by recommendations of site specific soils report. Therefore, impacts related to expansive soils will be less than significant.*

i) Unique geologic or physical features? (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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*Discussion: There are no unique geologic or physical features on or near the project site.*

**IV. WATER.** Would the proposal result in:

a) Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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*Discussion: The project sites are currently vacant, however, the project hardscape and landscaping features are designed to retain all surface drainage on site to minimize surface drainage runoff. Given the drainage design for the site, the project will only slightly increase the amount of surface runoff and decrease absorption rate, which will be less than significant.*

b) Exposure of people or property to water related hazards such as flooding? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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*Discussion: There is no potential to expose people or property to water related hazards due to this project since it is not in a flood zone.*



**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
<p>c) Discharge into surface waters or other alteration of surface water quality (e.g., temperature, dissolved oxygen or turbidity)? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: Water discharge from this project will ultimately be conveyed after short term detention, into the Salinas River. Water from the site will be required to be filtered through NPDES compliant and approved system to remove contaminants prior to discharge into surface water in compliance with the Urban Water Management Plan, such as the Salinas River, so that it does not result in degraded water quality.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>d) Changes in the amount of surface water in any water body? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: There is no water body on or near the project site.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e) Changes in currents, or the course or direction of water movement? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: This project could not result in changes in currents or water movement since there is no water course in the vicinity that could be affected by this project.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>f) Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of groundwater recharge capability? (Sources: 1,3, &amp; 7)</p> <p><i>Discussion: The proposed project does not directly withdraw water resources. The project is consistent with the build-out scenario in the General Plan and planned water use and reserve capacity.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>g) Altered direction or rate of flow of groundwater? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: This project could not result in alterations to the direction or rate of groundwater flow since this project does not directly extract groundwater or otherwise significantly affect these resources.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>h) Impacts to groundwater quality? (Sources: 1, 3, &amp; 7)</p> <p><i>Discussion: The project will not affect groundwater quality since this project does not directly extract groundwater or otherwise affect these resources, and the proposed uses do not utilize materials or methods that would result in reduced groundwater quality. This project will not change existing water quality from discharging in surface waters with implementation of standard storm water discharge infrastructure that is in compliance with the National Pollution Discharge Elimination System (NPDES) requirements.</i></p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>i) Substantial reduction in the amount of groundwater otherwise available for public water supplies? (Sources: 1, 3, &amp; 7)</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	Potentially Significant	No Impact
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*Discussion: Refer to response f.*

**V. AIR QUALITY.** Would the proposal:

- a) Violate any air quality standard or contribute to an existing or projected air quality violation? (Sources: 1, 3, & 7)

*Discussion: As an infill development site, in compliance with planned development for this zoning district previously anticipated and evaluated in the General Plan EIR, the project will not violate air quality standards.*

- b) Expose sensitive receptors to pollutants? (Sources: 1, 3, & 7)

*Discussion: There are no sensitive receptors such as schools, hospitals, etc. within the near vicinity that could be impacted by this project.*

- c) Alter air movement, moisture, or temperature? (Sources: 1, 3, & 7)

*Discussion: This project does not have the potential to significantly alter air movement, moisture, or temperature since the project is a small scale infill project.*

- d) Create objectionable odors?

*Discussion: Given the nature of the proposed uses, this project does not generally have the potential to create objectionable odors.*

**VI. TRANSPORTATION/CIRCULATION.** Would the proposal result in:

- a) Increased vehicle trips or traffic congestion? (Sources: 1, 3, & 7)

*Discussion: A Traffic Impact Study was prepared by Associated Transportation Engineers, November 2006. The study assessed the existing conditions, proposed project trip generation, and evaluated potential traffic impacts that may result from the project for the near-term with approved projects in the impact area. The study also evaluated impacts that may result from the project in the near-term with interim improvements. Additionally, the study evaluated impacts that may result from the proposed project in regard to the SB 46W/101 off-ramp queuing. It also assigned traffic contribution percentages (see Table 7) for approved projects (including the Durand Hotel project) for the purpose of assessing project cost for individual projects for the near-term improvements. See Attachment 2, Traffic Impact Study.*

*Background:*

*When this project was originally proposed (2 years ago), it was determined that the traffic impacts that may result from this project with other projects already approved in the vicinity, would exceed the adopted Level of Service (LOS) thresholds for the Hwy.101/46W interchange for both the City and Caltrans. Since then, interim improvements to the interchange including extending the S/B 101 offramp and other improvements (i.e. traffic signals, turn lanes, etc.) have been constructed. This specific improvement addresses safety issues of freeway queuing. Additionally, several projects that were previously entitled by the City have been eliminated from the approved projects list, since the City has acquired these properties for future long-term interchange improvements. However, since then regional traffic has increased, so*

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ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Potentially Significant Less Than Significant Impact	No Impact
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*that even with the properties/projects removed from the approved project list (and the associated trips generated that would impact the interchange), regional traffic has increased. Therefore, the project plus the current existing conditions and entitled projects, still exceeds the LOS for various weekday, Friday and Saturday peak hours for vehicle delays.*

*The General Plan and EIR identify interim and long-term improvements for the interchange. The City and Caltrans have completed the interim improvements, which has reduced safety queues and delays at the interchange. Additionally, the approved projects list has been shortened because the City has been implementing the long-term mitigation measures, and has been purchasing properties (with approved projects), thus taking trips out of the list of cumulative projects, for the purpose of using these properties for the long-term capacity increasing mitigation measure improvements. Additionally, the city is implementing long-term mitigation measures by assembling the Impact Assessment District which applies to all properties entitled within the interchange impact area, to participate in paying the projects per rata share of impacts.*

*Therefore, the City is in the process of mitigating impacts associated with development in the Hwy. 101/46W interchange. When the long-term improvements are completed (anticipated to be complete in 2009), the LOS at the interchange is projected to be within acceptable limitations of the adopted General Plan. Therefore, with mitigation measures incorporated (participation in the interchange assessment district) and upon implementation of improvements at the interchange by the city and Caltrans, this project will ultimately result in less than significant traffic impacts.*

- b) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (Sources: 1, 3, & 7)

*Discussion: The proposed project does not include road improvements that may result in safety hazards or in incompatible uses.*

- c) Inadequate emergency access or inadequate access to nearby uses? (Sources: 1, 3, & 7)

*Discussion: The project is adequately served for emergency services.*

- d) Insufficient parking capacity on-site or off-site? (Sources: 1, 3, 7, & 8)

*Discussion: Per the Zoning Ordinance parking requirements this project requires 155 parking spaces for the hotel use and 30 spaces for the cottages. The applicant has proposed 176 spaces for the hotel, and 48 spaces for the cottages, which exceeds the parking requirements. Therefore, the project has sufficient onsite parking capacity and will not require use of offsite parking.*

- e) Hazards or barriers for pedestrians or bicyclists? (Source: 7)

*Discussion: The project does not have hazards or barriers for pedestrians or bicyclists.*

- f) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Sources: 1 & 8)

*Discussion: The project would not conflict with or otherwise affect adopted policies supporting alternative*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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*transportation.*

- g) Rail, waterborne or air traffic impacts?

*Discussion: The project could not affect rail, waterborne or air traffic.*

**BIOLOGICAL RESOURCES.** Would the proposal result in impacts to:

- Endangered, threatened or rare species or their habitats (including but not limited to: plants, fish, insects, animals, and birds)?

*Discussion: There are no endangered, threatened or rare species or their habitats located on the project site. Although the site is vacant, it has been disturbed over time by being grubbed, used for stockpiling, etc. It is an urban infill site, surrounded by development (and Hwy. 101 to the east) on all side. Thus, there could not be potential impacts to endangered, threatened or rare species or their habitats.*

- b) Locally designated species (e.g., heritage trees)?

*Discussion: There are several oak trees on the project sites. Per the Arborist Report, all trees will be preserved and protected on the project sites, and specific mitigation measures are recommended for potential encroachment into oak tree driplines and critical root zones, and for landscaping. With tree protection measures and recommendations incorporated as mitigation measures, the project will result in less than significant impacts on the existing oak trees.*

- c) Locally designated natural communities (e.g., oak forest, coastal habitat, etc.)?

*Discussion: See item b. above.*

- d) Wetland habitat (e.g., marsh, riparian and vernal pool)?

*Discussion: There are no wetland habitats on or near the project site.*

- e) Wildlife dispersal or migration corridors?

*Discussion: The site is not part of a wildlife dispersal or migration corridor.*

**VIII. ENERGY AND MINERAL RESOURCES.** Would the proposal:

- a) Conflict with adopted energy conservation plans? (Sources: 1 & 7)

*Discussion: The structures will be designed and constructed according to applicable UBC codes and Title 24 energy conservation requirements, thus it will not conflict with adopted energy conservation plans.*

**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Use non-renewable resources in a wasteful and inefficient manner? (Sources: 1 & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project will not use non-renewable resource in a wasteful and inefficient manner.</i>				
c) Result in the loss of availability of a known mineral resource that would be of future value to the region and the residents of the State? (Sources: 1 & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project is not located in an area of a known mineral resources that would be of future value to the region and the residents of the State.</i>				

**IX. HAZARDS.** Would the proposal involve:

a) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project will not result in a risk of accidental explosion or release of hazardous substances since the uses do not generally use these types of substances.</i>				
b) Possible interference with an emergency response plan or emergency evacuation plan? (Sources: 1 & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project will not interfere with an emergency response plan or emergency evacuation plan since it is not a designated emergency response location to be used for staging or other uses in an emergency.</i>				
c) The creation of any health hazard or potential hazards?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project and future uses will not likely result in creating any health or other hazards.</i>				
d) Increased fire hazard in areas with flammable brush, grass, or trees?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Discussion: The project site is not located in an area with the potential for increased fire hazards. The site will be required to be in compliance with City and County brush and grass clearance requirements.</i>				

**X. NOISE.** Would the proposal result in:

a) Increases in existing noise levels? (Sources: 1, 7, & 8)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<i>Discussion: The project will not likely result in a significant increase in operational noise levels. It may result in short-term construction noise. However, construction noise will be limited to specific daytime hours per city regulations.</i>				

## 10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
b) Exposure of people to severe noise levels? (Source: 3)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

*The project site is not located in the vicinity where it would expose people to severe noise levels.*

### XI. PUBLIC SERVICES. Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Fire protection? (Sources: 1, 3, 6, & 7)                                | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Police Protection? (Sources: 1, 3, & 7)                                 | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Schools? (Sources: 1, 3, & 7)   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Maintenance of public facilities, including roads? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Other governmental services? (Sources: 1,3, & 7)                        | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

*Discussion: a.-e. The project applicant will be required to pay development impact fees as established by the city per AB 1600 to mitigate impacts to public services.*

### XII. UTILITIES AND SERVICE SYSTEMS. Would the proposal result in a need for new systems or supplies, or substantial alterations to the following utilities:

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Power or natural gas? (Sources: 1, 3, & 7)   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Communication systems? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Local or regional water treatment or distribution facilities? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Sewer or septic tanks? (Sources: 1, 3, 7, & 8)                                     | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Storm water drainage? (Sources: 1, 3, & 7)   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f) Solid waste disposal? (Sources: 1, 3, & 7)   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| g) Local or regional water supplies? (Sources: 1, 3, & 7)                             | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

*Discussion: a.-g. The project will not result in the need for new systems or supplies, or result in substantial alterations to utilities and service systems.*

### XIII. AESTHETICS. Would the proposal:

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Affect a scenic vista or scenic highway? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

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**10 Environmental Checklist Form**

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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*Discussion: The project is not located in a scenic vista or scenic highway area.*

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Have a demonstrable negative aesthetic effect?<br>(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: The project is proposed to be designed with high quality materials and architectural design that is suitable to the site and will complement the area, and will not have a demonstrable negative aesthetic effect.*

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c) Create light or glare? (Sources: 1, 3, 7, & 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: All light fixtures will be shielded and downcast as required per city regulations.*

**XIV. CULTURAL RESOURCES.** Would the proposal:

- |  |                          |                          |                                     |                                     |
|--|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Disturb paleontological resources? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| b) Disturb archaeological resources? (Sources: 1, 3, & 7)  | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |

*Discussion: a.-b. The project site is not located in an area with know paleontological or archaeological resources. If these types of resources are found during grading and excavation, appropriate procedures will be followed including halting activities and contacting the County Coroner, and follow standard mitigation procedures.*

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| c) Affect historical resources? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: There are no existing historical resources on the project site.*

- |   |                          |                          |                          |                                     |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| d) Have the potential to cause a physical change which would affect unique ethnic cultural values? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: The project is not proposed in a location where it could affect unique ethnic cultural values.*

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| e) Restrict existing religious or sacred uses within the potential impact area? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: Discussion: There are no known religious or sacred uses on or near the project site.*

**XV.RECREATION.** Would the proposal:

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Increase the demand for neighborhood or regional parks or other recreational facilities? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

*Discussion: The project will not significantly affect the demand for parks and recreational facilities. The project complies with the build-out scenario of the General Plan which has adopted park and recreation facility thresholds per population. The applicant will need to pay associated park in-lieu fees for the residential units.*

- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Affect existing recreational opportunities? (Sources 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

**10 Environmental Checklist Form**

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
ISSUES (and Supporting Information Sources):				

*Discussion: The project will not affect existing recreational opportunities.*

**MANDATORY FINDINGS OF SIGNIFICANCE.**

Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

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*Discussion: The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory.*

Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals? (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

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*Discussion: The project will not likely have a potential to achieve short-term, to the disadvantage of long-term environmental goals.*

Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.) (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

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*Discussion: The project will not result in significant cumulative impacts.*

Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly? (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
--------------------------	--------------------------	--------------------------	-------------------------------------

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*Discussion: The project will not result in substantial adverse environmental impacts on human beings, either directly or indirectly.*



## 11. EARLIER ANALYSIS AND BACKGROUND MATERIALS

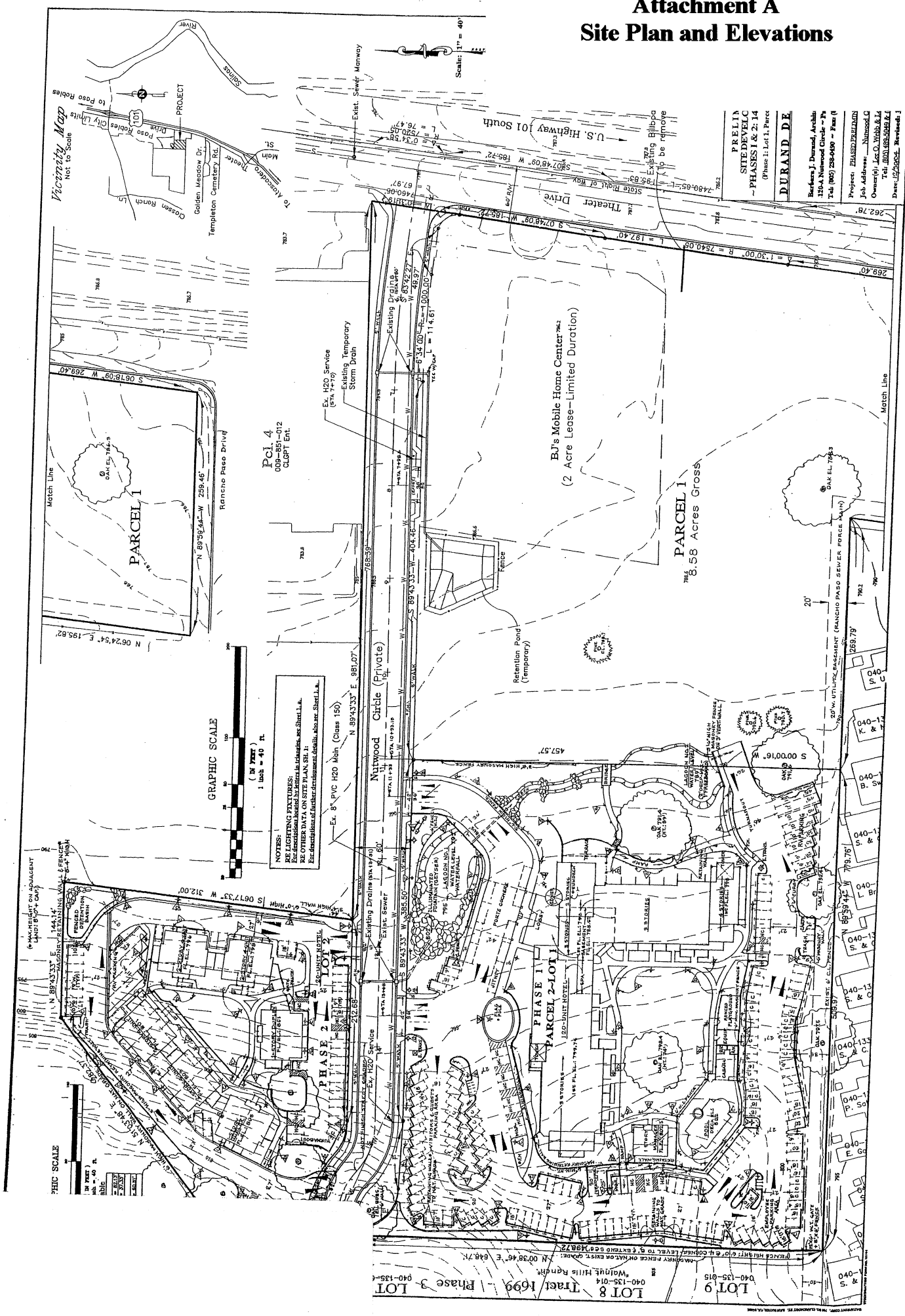
Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D). The earlier documents that have been used in this Initial Study are listed below.

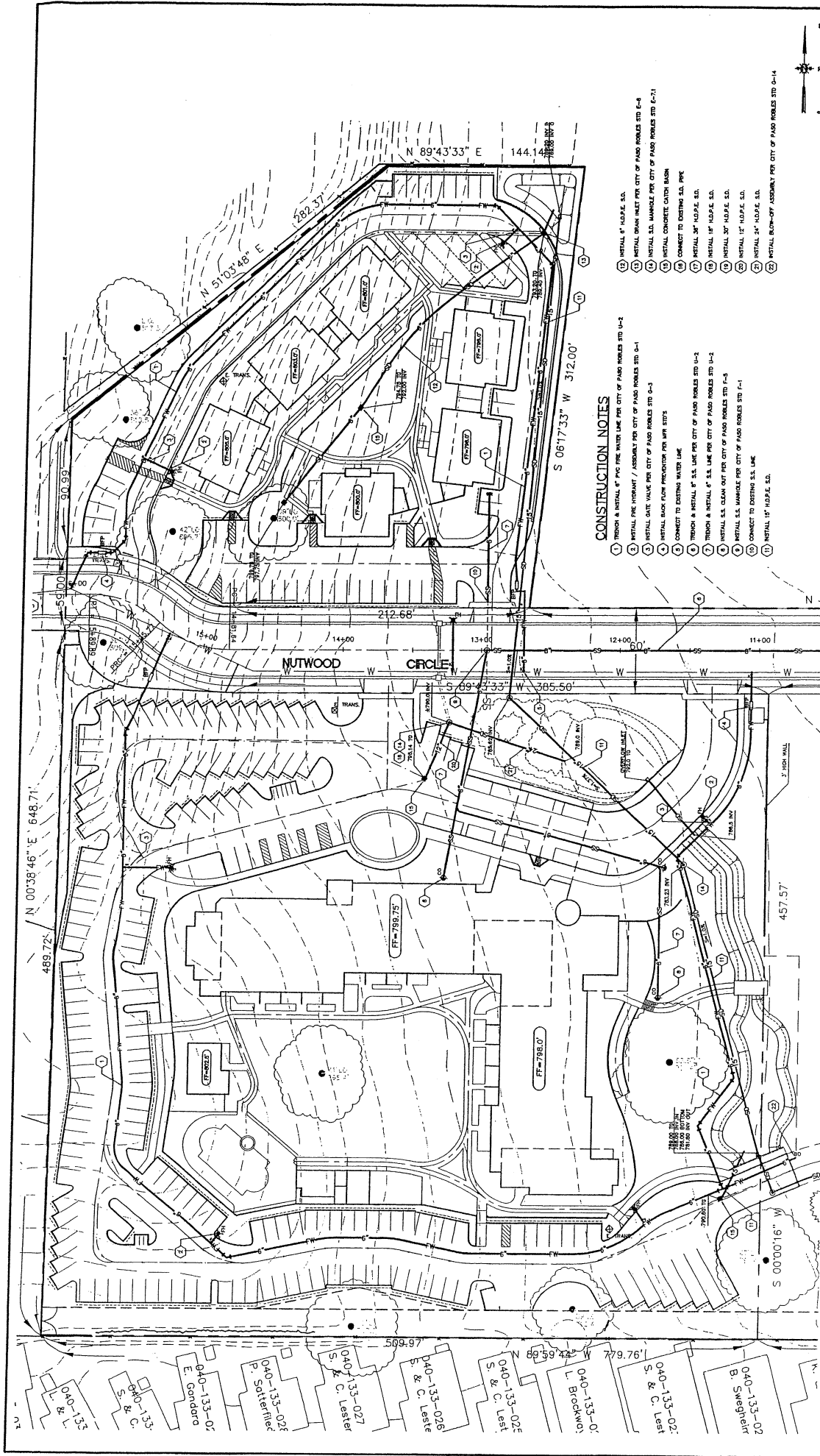
<b>Reference Number</b>	<b>Document Title</b>	<b>Available for Review At</b>
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
2	Seismic Safety Element for City of Paso Robles	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
3	Final Environmental Impact Report City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
4	Soil Survey of San Luis Obispo County, California Paso Robles Area	USDA-NRCS, 65 Main Street-Suite 108 Templeton, CA 93465
5	Uniform Building Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
6	City of Paso Robles Standard Conditions of Approval For New Development	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
7	City of Paso Robles Zoning Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
8	City of Paso Robles, Water Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
9	City of Paso Robles, Sewer Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
10	Federal Emergency Management Agency Flood Insurance Rate Map	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446

### **Attachments:**

Attachment A – Site Plan and Elevations  
Attachment B – Traffic Impact Study  
Attachment C – Arborist Report

# Attachment A Site Plan and Elevations





**CONSTRUCTION NOTES**

- 1) TROUGH & INSTALL 4" PVC FIRE WATER LINE PER CITY OF PASO ROBLES STD U-2
- 2) INSTALL FIRE HYDRANT / ASSEMBLY PER CITY OF PASO ROBLES STD U-1
- 3) INSTALL GATE VALVE PER CITY OF PASO ROBLES STD U-3
- 4) INSTALL BACKFLOW PREVENTOR PER MPR STDS
- 5) CONNECT TO EXISTING WATER LINE
- 6) TROUGH & INSTALL 4" S.S. LINE PER CITY OF PASO ROBLES STD U-2
- 7) TROUGH & INSTALL 4" S.S. LINE PER CITY OF PASO ROBLES STD U-2
- 8) INSTALL S.S. CLEAN OUT PER CITY OF PASO ROBLES STD U-3
- 9) INSTALL S.S. WASTE PER CITY OF PASO ROBLES STD U-1
- 10) CONNECT TO EXISTING S.S. LINE
- 11) INSTALL 1/2" HDPE S.S.
- 12) INSTALL 1" HDPE S.S.
- 13) METAL DRINK FOUNTAIN PER CITY OF PASO ROBLES STD E-4
- 14) INSTALL S.S. WASTE PER CITY OF PASO ROBLES STD E-2
- 15) INSTALL CONCRETE CATCH BASIN
- 16) CONNECT TO EXISTING S.S. PIPE
- 17) INSTALL 3/4" HDPE S.S.
- 18) INSTALL 1/2" HDPE S.S.
- 19) INSTALL 3/4" HDPE S.S.
- 20) INSTALL 1/2" HDPE S.S.
- 21) INSTALL 3/4" HDPE S.S.
- 22) INSTALL BLOW-OFF ASSEMBLY PER CITY OF PASO ROBLES STD Q-14

FOR S.S. SEE SHEET C50

FOR S.D. SEE SHEET C50

WEBB & MONSON HOTEL BY DURAND DESIGNS  
 PRELIMINARY UTILITY PLAN  
 CITY OF PASO DE ROBLES, CA

DATE: 7-5-08  
 DRAWN BY: [Signature]  
 CHECKED BY: [Signature]  
 SCALE: 1" = 30'

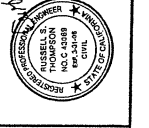
**RTIC**  
 R THOMPSON CONSULTING, INC.  
 ENGINEERING  
 1400 SOUTH MAIN  
 PASO ROBLES, CA 93271  
 (559) 255-1177

NO.	BY	DATE	DESCRIPTION

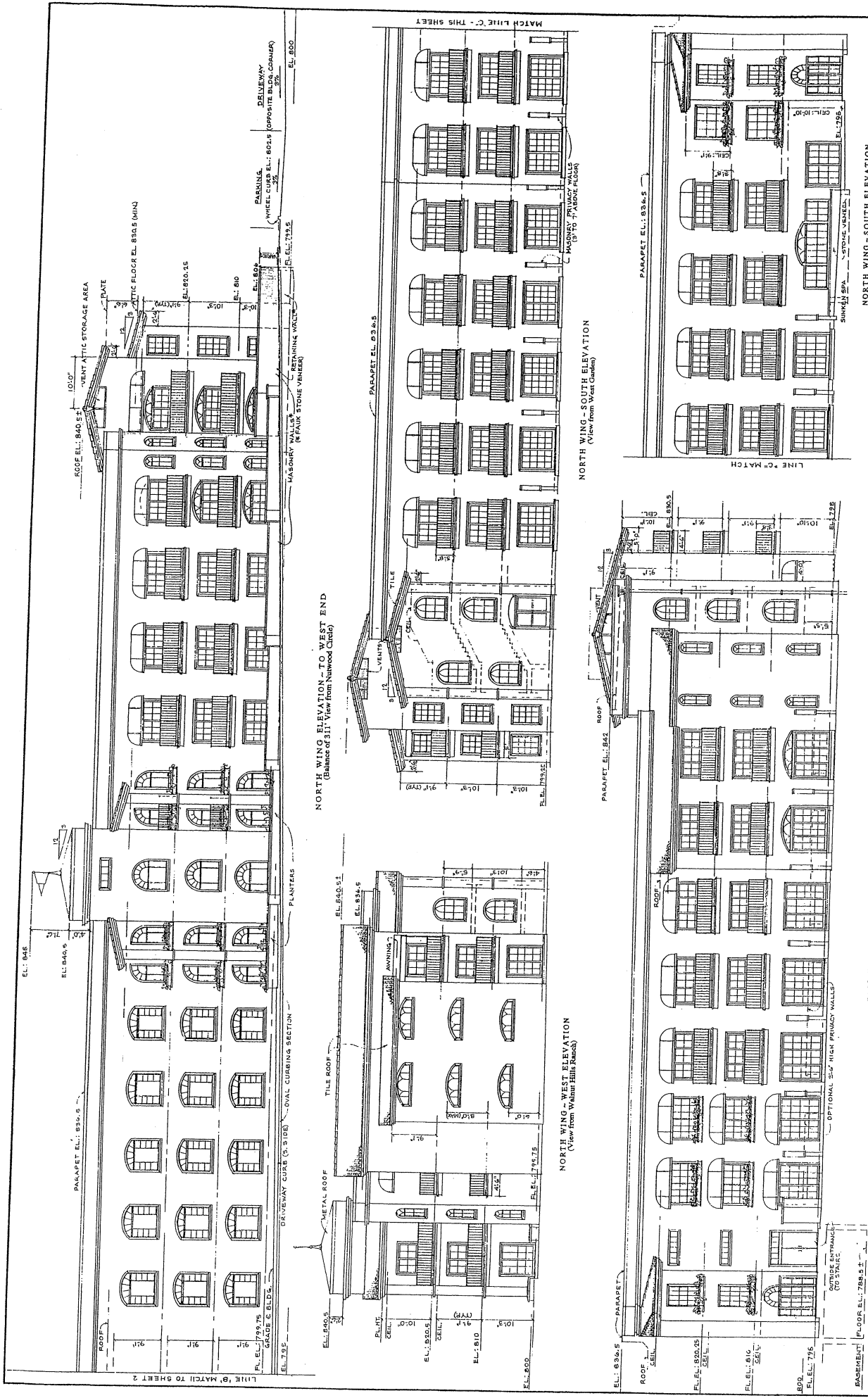
These plans and specifications, and the information contained herein, are prepared by the undersigned engineer or architect and are not to be construed as a contract or as a representation of the City of Paso Robles, California, without the written authority of the City Engineer or City Administrator.

Accepted by: [Signature]  
 City Engineer  
 Date: JUL 05 2008

City Engineer: [Signature]  
 Date: [Blank]





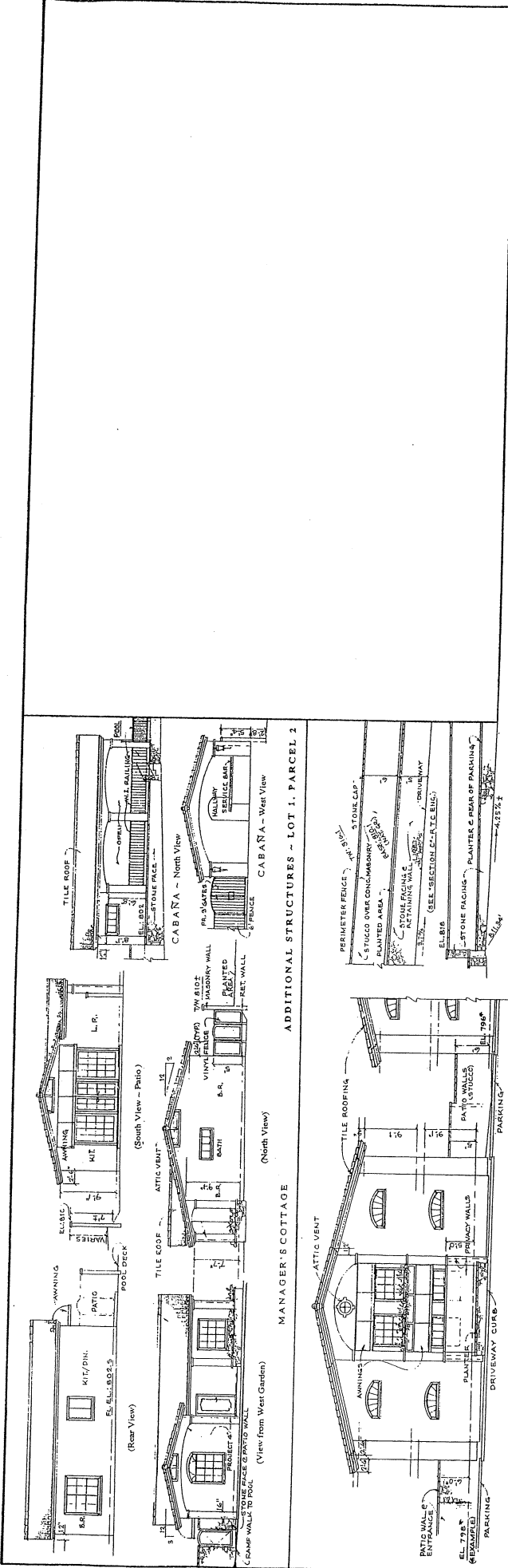


**PRELIMINARY DEVELOPMENT PLAN**  
 ~ HOTEL EXTERIORS: 120 UNITS ~  
 (Phase 1: Lot 1, Parcel 2 of Parcel Map F20-06-0471)  
 ~ Scale: 1/8" = 1' ~

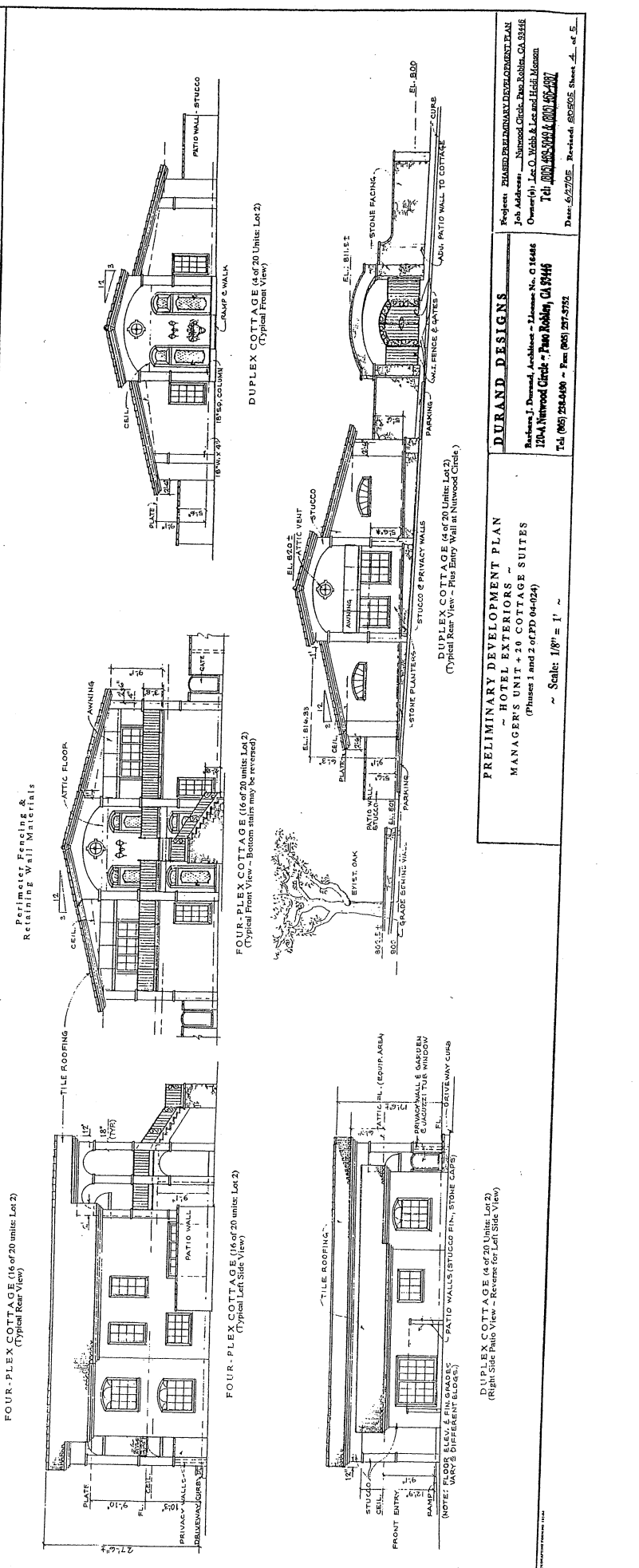
**DURAND DESIGNS**

Project: PHASE I PRELIMINARY DEVELOPMENT PLAN  
 Job Address: Nutwood Camp, West Garden, CA 92448  
 Owner(s): Lee O. Webb & Lee and Heidi Morrison  
 Tel: (951) 499-5438 & (909) 466-4827  
 Date: 8/27/05 Revised: 8/25/05 Sheet 3 of 5

Architect: J. Durand, Architect - License No. C 15488  
 1204 Nutwood Circle - Tustin, CA 92680  
 Tel: (951) 254-0480 • Fax: (951) 257-5752



ADDITIONAL STRUCTURES - LOT 1, PARCEL 2



**PRELIMINARY DEVELOPMENT PLAN**  
 ~ HOTEL EXTERIORS ~  
 MANAGER'S UNIT - 20 COTTAGE SUITES  
 (Phases 1 and 2 of PD 04-124)  
 ~ Scale: 1/8" = 1' ~

**DURAND DESIGNS**  
 Barbara J. Durand, Architect - License No. 016486  
 1204 Nitwood Circle - Palo Alto, CA 94304  
 Tel: (650) 995-5949 & (650) 995-2007  
 Fax: (650) 255-0400 - Fax: (650) 257-5752

Project: PALM BEACH RESORT DEVELOPMENT PLAN  
 Job Address: Nitwood Circle, Palo Alto, CA 94304  
 Owner(s): J.C. O'Neil & Lee and Heidi Momen  
 Date: 6/27/05 - Revised: 8/28/05, Sheet 2 of 5



**ASSOCIATED TRANSPORTATION ENGINEERS**

100 N. Hope Avenue, Suite 4, Santa Barbara, CA 93101

Richard L. Pool, P.E.  
Scott A. Schell, AICP

Paso Robles

NOV 14 2006

Planning Division

06113R01.WPD

November 13, 2006

John Falkenstien  
City of Paso Robles  
1000 Spring Street  
Paso Robles, CA 93446

***TRAFFIC IMPACT STUDY FOR THE DURAND PROJECT, CITY OF PASO ROBLES***

Associated Transportation Engineers (ATE) has prepared the following traffic study for the Durand Project, proposed in the southern portion of the City of Paso Robles. The project is proposing to construct a 120-room hotel on the southwest corner of the Theater Drive/Nutwood Circle intersection plus 20 hotel guest suites across the street on Nutwood Circle.

**EXISTING CONDITIONS**

Table 1 shows the Existing Weekday Midday, Weekday P.M., Friday P.M., and Saturday P.M. peak hour levels of service for the intersections comprising the Route 46 (W)/Route 101 interchange. Peak hour traffic volumes for the interchange were collected in April and June 2005. The counts include the number of trucks using the interchange.

**Table 1**  
**Route 46 (W)/Route 101 Interchange**  
**Existing Peak Hour Levels of Service**

Intersection	Seconds Delay Per Vehicles/LOS			
	Weekday Midday	Weekday P.M.	Friday P.M.	Saturday P.M.
Rt 46(W)/Theater-Vine/Rt 101 SB	34.1/LOS C	35.6/LOS D	38.8/LOS D	36.4/LOS D
Rt 46(W)/Rt 101 NB	14.5/LOS B	14.6/LOS B	19.7/LOS B	17.7/LOS B
Rt 46(W)/Ramada	15.8/LOS C	15.5/LOS C	18.6/LOS B	14.2/LOS B

LOS based on average delay per vehicle in seconds pursuant to the Highway Capacity Manual Operations Methodology. LOS for Rt 46(w)/Theater-Vine and Rt 46(w)/Rt 101 SB are based on average delay per vehicle for all movements using the two intersections since they operate as a single unit.

The Route 46 (W)/Theater-Vine/Route 101 SB intersection operates at LOS C during the Weekday Midday period and LOS D during the Weekday P.M., Friday P.M., and Saturday P.M. peak hour periods. The two intersections comprising the east side of the interchange operate at LOS B-C.

### NEAR-TERM CONDITIONS

Near-term traffic conditions were forecasted for the interchange assuming the additional traffic generated by the approved developments in the vicinity of the interchange, along with the near-term improvements planned for the interchange. The following section details the near-term level of service forecasting for the interchange.

### Approved Projects

There are several approved projects that will add traffic to the study-area street system in the near-term. Table 2 shows the approved projects in the area.



**Table 2**  
**Route 46 (W)/Route 101 Interchange - Approved Projects**

Project	Land Use	Size
Target Center <sup>a</sup>	Shopping Center	17,000 SF
La Vorgna Storage	Storage	2.8 Acres
McDonalds/Chevron	Fast-Food Restaurant Gas Station w/ Car Wash	3,152 SF 12 Pumps/1 Wash
Gheza Mini-Storage	Storage	3.75 Acres
Theater Drive Retail	Mixed Retail Self-Storage	39,650 SF 62,000 SF
Bellesara Suites Project	Lodging	62-Unit Hotel
1331 Vendels Circle - Benny Simmons	Light Industry	6,000 SF
1160 Ramada Drive - True Tube	Light Industry	6,900 SF
1375 Ramada Drive - Lavorgna	Light Industry	46,684 SF
1500 Ramada Drive - Pokrajac	Light Industrial/Warehouse	42,800 SF
The Inns at Vintner's Village Project	Lodging	138 rooms

<sup>a</sup> Remaining square footage under original shopping center approval.

### **Route 46 (W)/Route 101 Improvements**

The City commissioned a traffic study of the interchange in the Summer of 2002. That study analyzed traffic conditions at the interchange and included improvements to provide additional capacity at the interchange in the near term. The near-term improvements that are being constructed and are expected to be completed in late 2006-early 2007 include:

- 1) Modify Route 101 SB Off-Ramp. Lengthen the ramp storage lanes to provide 500-550 feet of storage. The off-ramp contains one left-turn lane and two right-turn lanes. The inside right-turn lane is designated for southbound Theater Drive and the outside lane is designated for Route 46 (W) & Vine Street.
- 2) Modify Curb Return on Northeast Corner of SB Off-Ramp. Modify the curb return to accommodate truck turns.

**SUMMARY**


The projected traffic from the existing plus approved projects will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday

P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

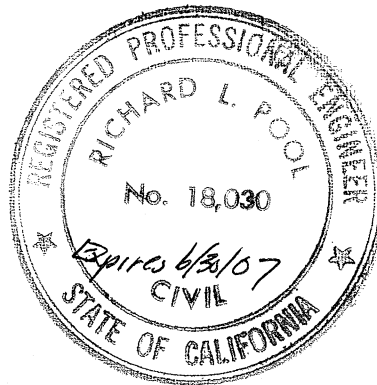
The addition of the projected Durand Project traffic will result in the Route 46(W)/Theater-Vine/Route 101 SB intersection operating at LOS D during the Weekday Midday, Weekday P.M., and Saturday P.M. peak hour periods, and LOS E during the Friday P.M. peak hour period. The Route 46 (W)/Route 101 NB/Ramada intersection is forecast to operate at LOS B during the Weekday Midday and Weekday P.M. peak hour periods, and LOS C during the Friday P.M. and Saturday P.M. peak hour periods.

The Durand Project's portion of the traffic that will be added to the interchange by the approved project is 8.3%.

Associated Transportation Engineers



By: Richard L. Pool, P.E.  
President



RLP/DLD/LDH

attachments

# A & T ARBOR


P.O. BOX 1311 TEMPLETON, CA 93465

## Tree Preservation Plan Addendum Durand Project Paso Robles, CA

8-01-05

Prepared by A & T Arborists  
and Vegetation Management

Chip Tamagni  
Certified Arborist #WE 6436-A



Steven Alvarez  
Certified Arborist #WE 511-A

RECEIVED  
AUG 05 2005  
Community Development

Tract # \_\_\_\_\_

PD # 04-024

Building Permit # \_\_\_\_\_

**Project Description:** This project involves the construction of a hotel, cottages and accompanying parking areas located off theater drive in Paso Robles, CA. The area currently has nine valley oak trees (*Quercus lobata*) and none are planned for removal. There are two additional valley oaks located on the property adjacent to lot #2.

**Specific Mitigations Pertaining to the Project:** As outlined on the spreadsheet, all trees must have protective fencing in place and inspected by the arborists prior to grading. All impacts (grading for curbing, parking, retaining walls and footings) shall be monitored and any necessary root pruning, wetting, root fungicide application will be completed by the project arborists. The arborists shall be called a minimum of 48 hours before any needed monitoring. The arborists shall also be called out to monitor the retaining wall trenching near the neighboring trees near lot #2. The crz encroachment for tree #9 should not impact the tree long-term. The grade for this tree shall not be changed within the fencing nor shall any grading contribute to a ponding condition around the trunk.

The term “critical root zone” or CRZ is an imaginary circle around each tree. The radius of this circle (in feet) is equal to the diameter (in inches) of the tree. For example, a 10 inch diameter tree has a critical root zone with a ten foot radius from the tree. Working within the CRZ usually requires mitigations and/or monitoring by a certified arborist.

All trees potentially impacted by this project are numbered and identified on both the grading plan and the spreadsheet. Trees are numbered on the grading plans and in the field with an aluminum tag. Tree protection fencing is shown on the grading plan. In the field oak trees to be saved have yellow tape attached to the tag (no removals for this project). Both critical root zones and drip lines are outlined on the plans.

If pruning is necessary for building, road or driveway clearance, removal of limbs larger than 6 inches in diameter will require a city approved permit along with a deposit paid in advance (to the City of Paso Robles). The city will send out a representative to approve or deny the permit. Only 25% of the live crown may be removed. At this time, no trees appear to need any building clearance pruning.

### **Tree Rating System**

A rating system of 1-10 was used for visually establishing the general health and condition of each tree on the **spreadsheet**. The rating system is defined as follows:

<u>Rating</u>	<u>Condition</u>
0	Deceased
1	Evidence of massive past failures, extreme disease and is in severe decline.
2	May be saved with attention to class 4 pruning, insect/pest eradication and future monitoring.

- 3 Some past failures, some pests or structural defects that may be mitigated by class IV pruning.
- 4 May have had minor past failures, excessive deadwood or minor structural defects that can be mitigated with pruning.
- 5 Relatively healthy tree with little visual, structural and/or pest defects and problems.
- 6 Healthy tree that probably can be left in its natural state.
- 7-9 Has had proper arboricultural pruning and attention or have no apparent structural defects.
- 10 Specimen tree with perfect shape, structure and foliage in a protected setting (i.e. park, arboretum).

Aesthetic quality is defined as follows:

- **poor** - tree has little visual quality either due to severe suppression from other trees, past pruning practices, location or sparse foliage
- **fair** - visual quality has been jeopardized by utility pruning/obstructions or partial suppression and overall symmetry is average
- **good** - tree has good structure and symmetry either naturally or from prior pruning events and is located in an area that benefits from the trees position
- **excellent** - tree has great structure, symmetry and foliage and is located in a premier location. Tree is not over mature.

All trees on the site are relatively the same size and age (mature). Aesthetically speaking, the project has been designed to positively accent the trees. They all rate from good to excellent. The trees on site have a useful life expectancy of 50 to 80 years.

The following mitigation measures/methods must be fully understood and followed by anyone working within the critical root zone of any native tree. Any necessary clarification will be provided by us (the arborists) upon request.

1. It is the responsibility of the **owner or project manager** to provide a copy of this tree protection plan to any and all contractors and subcontractors that work within the critical root zone of any native tree and confirm they are trained in maintaining fencing, protecting root zones and conforming to all tree protection goals. It is highly recommended that each contractor sign and acknowledge this tree protection plan.

2. Any future changes (within the critical root zone) in the project will need Project Arborist review and implementation of potential mitigation measures before any said changes can proceed.

3. **Fencing:** The proposed fencing shall be shown in orange ink on the grading plan. It must be a minimum of 4' high chain link, snow or safety fence staked (with posts 8 feet on center) at the edge of the critical root zone or line of encroachment for each tree or group of trees. The fence shall be up before any construction or earth moving begins. The owner shall be responsible for maintaining an erect fence throughout the construction period. The arborist(s), upon notification, will inspect the fence placement once it is erected. After this time, fencing shall not be moved without arborist inspection/approval. If the orange plastic fencing is used, a minimum of four zip ties

shall be used on each stake to secure the fence. All efforts shall be made to maximize the distance from each saved tree. Weather proof signs shall be permanently posted on the fences every 50 feet, with the following information:

**Tree Protection Zone**  
No personnel, equipment,  
materials, and vehicles are  
allowed  
Do not remove or re-position  
this fence without calling:  
A & T Arborists  
434-0131

4. **Soil Aeration Methods:** Soils within the critical root zone that have been compacted by heavy equipment and/or construction activities must be returned to their original state before all work is completed. Methods include water jetting, adding organic matter, and boring small holes with an auger (18" deep, 2-3' apart with a 2-4" auger) and the application of moderate amounts of nitrogen fertilizer. The arborist(s) shall advise.
5. **Chip Mulch:** All areas within the critical root zone of the trees that can be fenced shall receive a 4-6" layer of chip mulch to retain moisture, soil structure and reduce the effects of soil compaction.
6. **Trenching Within Critical Root Rone:** All trenching within the critical root zone of native trees shall be **hand dug**. All major roots shall be avoided whenever possible. All exposed roots larger than 1" in diameter shall be clean cut with sharp pruning tools and not left ragged. A **Mandatory** meeting between the arborists and grading contractor(s) must take place prior to work start.
7. **Grading Within The Critical Root Zone:** Grading should not encroach within the critical root zone unless authorized. Grading should not disrupt the normal drainage pattern around the trees. Fills should not create a ponding condition and excavations should not leave the tree on a rapidly draining mound.
8. **Exposed Roots:** Any exposed roots shall be re-covered the same day they were exposed. If they cannot, they must be covered with burlap or another suitable material and wetted down 2x per day until re-buried.
9. **Paving Within The Critical Root Zone:** Pervious surfacing is preferred within the critical root zone of any native tree. If pavers are required, the areas are outlined on the grading plans. Pavers must be interlocking with a minimum of 10% void space backfilled with pea gravel. Geo textile fabric shall be permeable. Depending on use within the CRZ, pavers may or may not be required. Parking spots do not require pavers as they provide a leaching area for oil and antifreeze. The non-parking area within the crz of tree #5 will require pavers as described above.
10. **Equipment Operation:** Vehicles and all heavy equipment shall not be driven under the trees, as this will contribute to soil compaction. Also there is to be no parking of equipment or personal vehicles in these areas. All areas behind fencing are off limits unless pre-approved by the arborist.

**11. Existing Surfaces:** The existing ground surface within the critical root zone of all oak trees shall not be cut, filled, compacted or pared, unless shown on the grading plans **and** approved by the arborist.

**12. Construction Materials And Waste:** No liquid or solid construction waste shall be dumped on the ground within the critical root zone of any native tree. The critical root zone areas are not for storage of materials either.

**13. Arborist Monitoring:** An arborist shall be present for selected activities (trees identified on spreadsheet and items bulleted below). The monitoring does not necessarily have to be continuous but observational at times during these activities. It is the responsibility of the **owner(s) or their designee** to inform us prior to these events so we can make arrangements to be present. All monitoring will be documented on the field report form which will be forwarded to the project manager and the City of Paso Robles Planning Department. Monitoring is \$75.00/hour, 2 hour minimum. A contract shall be signed prior to work start.

- pre-construction fence placement inspection
- all grading and trenching identified on the spreadsheet
- any other encroachment the arborist feels necessary

**14. Pre-Construction Meeting:** An on-site pre-construction meeting with the Arborist(s), Owner(s), Planning Staff, and the earth moving team shall be required for this project. Prior to final occupancy, a letter from the arborist(s) shall be required verifying the health/condition of all impacted trees and providing any recommendations for any additional mitigation. The letter shall verify that the arborist(s) were on site for all grading and/or trenching activity that encroached into the critical root zone of the selected native trees, and that all work done in these areas was completed to the standards set forth above.

**15. Pruning** Class 4 pruning includes-Crown reduction pruning shall consist of reduction of tops, sides or individual limbs. A trained arborist shall perform all pruning. No pruning shall take more than 25% of the live crown of any native tree. Any trees that may need pruning for road/home clearance shall be pruned **prior** to any grading activities to avoid any branch tearing.

**16. Landscape:** All landscape within the critical root zone shall consist of drought tolerant or native varieties. Lawns shall be avoided. All irrigation trenching shall be routed around critical root zones, otherwise above ground drip-irrigation shall be used. It is the owner's responsibility to notify the landscape contractor regarding this mitigation.

**17. Utility Placement:** All utilities, sewer and storm drains shall be placed down the roads and driveways and when possible outside of the critical root zones. The arborist shall supervise trenching within the critical root zone. **All trenches in these areas shall be exposed by air spade or hand dug with utilities routed under/over** roots larger than 3 inches in diameter.

**18. Fertilization and Cultural Practices:** As the project moves toward completion, the arborist(s) may suggest either fertilization and/or mycorrhiza applications that will benefit tree health. Mycorrhiza offers several benefits to the host plant, including faster growth, improved nutrition, greater drought resistance, and protection from pathogens.

The included spreadsheet includes trees listed by number, species and multiple stems if applicable, scientific name, diameter and breast height (4.5'), condition (scale from poor to excellent), status (avoided, impacted, removed, exempt), percent of critical root zone impacted, mitigation required (fencing, root pruning, monitoring), construction impact (trenching, grading), recommended pruning and individual tree notes.

If all the above mitigation measures are followed, we feel there will be no long-term significant impacts to the native trees.

Please let us know if we can be of any future assistance to you for this project.

Steven G. Alvarez  
Certified Arborist #WC 0511

Chip Tamagni  
Certified Arborist #WE 6436-A



DURAND DESIGNS  
NUTWOOD CIRCLE  
HOTEL SITE

TREE PROTECTION SPREAD SHEET

TREE #	TREE SPECIES	TRUNK DBH	TREE CONDITION	CONST STATUS	DRIPLINE % IMPACT	CONST IMPACT	MITIGATION		MONT REQUIRED	PRUNING CLASS	FIELD NOTES
							PROPOSAL	REQUIRED			
1	0	0	0	0	0%	0	0	0	0	0	
2	0	0	0	0	0%	0	0	0	0	0	
3	VO	47"	8	I	50%	G,C	F,M	Y	Y	C-4	MOVE VEH PKING, NAT PLTS
4	VO	53.5"	6	I	40%	G,C	F,M	Y	Y	C-4	LAGOON ADJUSTMENT?
5	VO	39"	4	I	40%	G,C	F,M	Y	Y	C-4	RD,PK, TRASH,PAVERS
6	VO	40"	4	I	30%	G,C	F,M	Y	Y	C-4	PK, DEADWD PRUNING A MUST*
7	VO	41"	6	I	100%	G,C	F,M	Y	Y	NONE	NO IRR,NO LAWN,NATIVE ONLY
8	VO	39"	4	I	15%	G,C	F,M	Y	Y	NONE	PARKING, CURB
9	VO	39"	5	I	80%	G,C	F,M	Y	Y	NONE	PK, CONST, GRADING
10	VO	42"	4	I	80%	G,C	F,M	Y	Y	NONE	PK, CONST, GRADING
11	VO	36	3	I	20%	G,C	F,M	Y	Y	NONE	PK, GRADING, CONST

→ 50% is landscape impact (drought tolerant)

- 1 = TREE #: MOSTLY CLOCKWISE FROM DUE NORTH
- 2 = TREE TYPE: COMMON NAME IE. W.O. = WHITE OAK
- 3 = TRUNK DIAMETER @ 4'
- 4 = TREE CONDITION: 1 = POOR, 10 = EXCELLENT
- 5 = CONSTRUCTION STATUS: AVOIDED, IMPACTED, REMOVAL
- 6 = DRIPLINE: PERCENT OF IMPACTED DRIPLINE
- 7 = CONSTRUCTION IMPACT TYPE: GRADING, COMPACTION, TRENCHING
- 8 = MITIGATION REQUIREMENTS: FENCING, MONITORING, ROOTING PRUNING, PRUNING
- 9 = ARBORIST MONITORING REQUIRED: YES/NO
- 10 = PERSCRIBED PRUNING: CLASS 1-4
- 11 = FIELD NOTES

\*TREE # 6 HAS THREE LIMBS THAT ARE HAZARDIST AND OVER HANGING THE NEIGHBORING TRAILOR PARK THE LIMBS SHOULD BE REMOVED AS SOON AS POSSIBLE

## Hotel Project on Nutwood Circle,

The two principals involved with this hotel project are Lee Webb and Lee Monson.

Lee Webb has resided in Southern California for more than 25 years. Lee Webb is a seasoned real estate broker with significant experience in the building and managing a number of upscale hotel and motels including several in Southern California and more recently the Casa Grande Hotel in Arroyo Grande.

Lee Monson is a 4th generation native of San Luis Obispo County and a graduate of Templeton High School. He holds an MBA degree in Finance. He and his wife Heidi are 30+ year residents of Atascadero.

### **The Following Is Being Presented For Your Consideration And Approval:**

An upscale 140-room hotel in the City of Paso Robles. The architectural theme will resemble that found in the Provence region of Southern France. The hotel will consist of a 120 room main hotel located on 5.34 acres on the south side of Nutwood Circle with an additional 20 Special Guest Suites located on 1.95 acres across the street from the Hotel on the north side of Nutwood circle.

### THE MAIN HOTEL

120 Room, Multi-Story approximately 90,000 square feet will encompass the following facilities;

#### **Building Area:**

First Floor: Spacious Lobby, Night Auditor's Office, Marketing Director's Office, Manager's Office, Check - in Counter, Holistic Beauty, Health Spa and Sauna

Second Floor Deluxe Rooms

Third Floor Deluxe Rooms (including roof garden)

Fourth Floor (Penthouse Suites) Auxiliary Building

Basement (Laundry, utilities, equipment, wine cellar)

**Guest Rooms:** Proposed are 100 comfortable deluxe guest rooms plus 20 suites, some of which will include private bedrooms. All rooms will be equipped with the latest TV and computer ports, a coffee maker, hair dryer, etc. Selected rooms will also have private patios or balconies, Jacuzzi tubs, a desk, a small refrigerator and a wet bar. Handicap access rooms shall meet or exceed the minimum numbers required.

#### **General Building, Facility Features And Services:**

**Architecture:** A French style resembling that of southern Provence.

**Hotel Entrance:** both entry driveways are accented by striking landscaping, including a lighted fountain which spills into a pond creating a waterfall splashing into a lighted lagoon. A drain

under the east driveway will feed a live creek to flow into a lower lagoon. The water features will connect to an overflow outlet which will extend to Theatre Drive and under it to an existing culvert.

**Lobby Features:** Guest check - in counter and relaxation area; tour information booth and tour gathering area; public restrooms, stair access to the second floor, Conference/Banquet areas, Access to the Breakfast Cafe, elevators and "Stair Tower;"

**Stair Tower:** Second access to Conference/Banquet rooms, Roof Garden and 4th level lookout.

**Elevators:** Total of 4 (2 in Lobby and 1 at each end of the guest wings.) "4-stop" elevators will provide access to the basement and the 3rd floor and from the 1st floor to the 4th floor and roof area.

**Breakfast Cafe:** This kitchen and dining area is accessible from the Lobby and the corridors, linking both east and west garden areas, as well as the guest room corridors. Guest will enjoy a daily breakfast buffet, special breakfast on weekends, and hosting a special afternoon or evening teas or wine events. A self-service coffee bar will be available all day and an evening wine bar will be hosted on weekends. Sales of local wines will come from a basement wine cellar.

**Holistic Beauty, Health Spa and Sauna:** Located In a prominent area will be a fashionable beauty salon and health spa that will cater to the latest skin and beauty treatments.

**Meeting/Banquet Areas:** A 290 sf meeting room (seating 24), is located off the Lobby for use by guests or hotel staff. A 1,440 sf meeting room on the second floor (above the Lobby, adjoining the east terrace) will seat 80 persons at a banquet or 150 at other events. A caterer's kitchen and storage area will adjoin the west end of this room. Folding doors may be used to divide this room into 2 equal spaces.

**Roof Garden/Sun Deck:** Located on the third floor level and accessed by elevator or stairs. this area is to be available to quests for either private or public events. An exercise room. with private massage alcove, may adjoin the open deck, which will be landscaped and furnished with tables, chairs, umbrellas, etc.

**Outdoor Gardens:** Two large outdoor areas will provide attractive venues for quest and party groups, with direct access to adjoining guest rooms and guest parking. The West Garden includes a swimming pool and Jacuzzi, cabana with rest rooms, and a fenced playground. The East Garden includes a terrace near the Breakfast Cafe access to the corridor linking the two Gardens and pedestrian access to the landscaped grounds and Lagoon No.2, and a pleasant easterly view of the hills across the Salinas River.

**Personal Services:** Such as child care or pet care may also be made available.

**Parking:** Hotel Guests & visitor parking: 152 spaces (12 Handicap and 10 oversized spaces); Hotel Jitney: 1; Vendors 2. Employee Parking: 18 (1 per employee per shift) total Parking spaces 173. (Temporary check - ins by bus or car, and motorcycle and hotel cars parking spaces are not

included in this total)

## **SPECIAL GUEST SUITES**

**Description of Facilities:** 20 Suites in 6 Cottages, two Single Story & six 2 Story

**Building Area:** The two single story cottages are to be designed for the handicapped. As depicted, each of these suites will occupy 1,138 sf or 2,276 sf in each building. The six 2 Story building will be occupied by 4 suites: first floor suite: 1,104 sf each; Second floor suite: 968 sf each (+ 136 sf balcony); Total of each 2 Story building: 4,144 sf. Total Estimated Building Area: 21,128 sf

**Special Guest Suites** will serve those hotel guests that desire larger, exclusive accommodations and the option to reserve a stay of one week or up to 30 days. The group of six (6) buildings will consist of two (2) one story duplexes (handicapped accessible), and four (4) two story buildings with 4 suites each. These guests may reserve privileges to all or a selected portion of the amenities enjoyed by the occupants of the main hotel.

**Architecture:** The theme will match that of the 120 room hotel but portrayed as a small French village. Entrance to the "Village:" Pedestrians, guests and visitors will enter through large wrought iron gates, adjoined by 8' high stone walls, giving more exclusivity to this portion of the hotel complex.

**Special Guest Suite Features:** Each unit will provided for at least 4 persons and will include a kitchen and bar, a fireplace and spa, laundry facilities, and fenced patios or balconies, as well as the television and computer ports similar to those provided to the 20 suites in the Main Hotel. All suites will overlook the central garden.

**Personal Services:** Guests will have access to the same services provided to those In the Main Hotel. 4

**Outdoor Gardens:** A central garden will be accessible form alt suites. Landscaping will include at least one water feature and outdoor seating, as well as artistic landscaping.

**Parking:** Each suite will have 1 dedicate parking space, some next to the suite. Other dedicated spaces will be nearby, providing at least 1.5 spaces per suite. This results in the following distribution:

**Total Parking Spaces:** 55 (including 8 handicap)(RV spaces are 11' x 40' to 45')

The vision is that this hotel will be a gem of Paso Robles, attracting the most discriminating of travelers to visit again and again. It will be more than just a nice, comfortable place to stay, but a select retreat where one can come to relax and utilize its location as a base of operations from which to venture out, explore, and take pleasure in the many and varied attractions offered by Paso Robles and the surrounding areas.